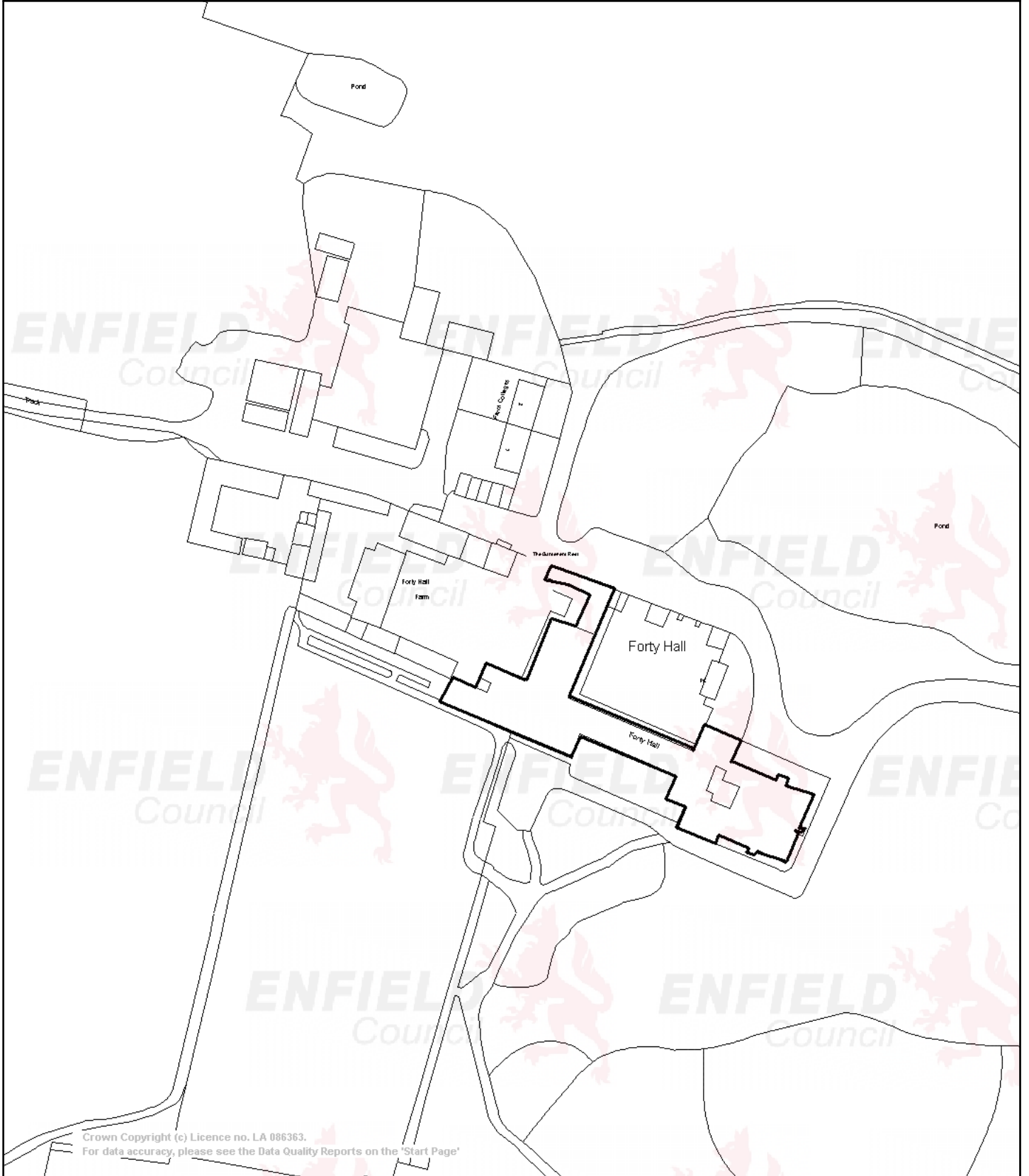


PLANNING COMMITTEE - 17th December 2009

LBC/09/0019



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Scale 1/1250 Date 1/12/2009

Centre = 533627 E 198581 N

Application Number: LBC/09/0019
Date of Registration: 26th August 2009

Ward: Chase

Contact: Jane Tebbutt 3849

Location: FORTY HALL, FORTY HILL, ENFIELD, EN2 9EU

Proposal: External and internal repairs and alterations involving replacement of main staircase, installation of lift shaft, reconfiguration of back staircase, removal of entrance porch within central courtyard and construction of glazed roof to central courtyard.

Applicant Name & Address:

Ms Liz Cody, London Borough of Enfield
CIVIC CENTRE
SILVER STREET
ENFIELD
EN1 3XA

Agent Name & Address:

Mr Brian Lofthouse, Thomas Ford and Partners
177, Kirkdale
Sydenham
SE26 4QH

Recommendation: That in accordance with Regulation 13 of the Planning (Listed Buildings and Conservation Areas) Regulations 1990 the Director of Education, Children's Services and Leisure be invited to make an application to the Secretary of State for Communities and Local Government who should be invited to attach the following conditions to any approval: -

1. That development shall not commence until details of the new lift as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The new lift shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

2. That development shall not commence until details of the courtyard roof as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The courtyard roof shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

3. That development shall not commence until details of the replacement staircase as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The new staircase shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

4. All new works and works of repair and reinstatement shall match exactly the existing adjacent work with regard to the methods used and to material, colour, texture, profile and workmanship and in the case of brickwork, facebond and pointing, unless shown otherwise on the drawing or other documentation hereby approved or required by a condition attached to this consent.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the Listed Building.

5. No development shall take place until the applicant has secured the implementation of a programme of archaeological work, in accordance with a written scheme of investigation which has been submitted by the applicant and approved by the Local Planning Authority.

Reason: To safeguard archaeological heritage.

6. That development shall not commence until the applicant has recorded to English Heritage standards all areas of the building to be repaired or altered. The scheme of recording to be submitted to and approved by the Local Planning Authority.

Reason: To ensure that a full record is retained of the building's historical heritage.

7. C53A Time Limit - Listed Building Consent

Site and surroundings

Forty Hall is a grade I listed building situated within Forty Hill and Bulls Cross Conservation Area, the Green Belt and Area of Special Character. It is also included on the National Register of Historic Parks and Gardens. The Forty Hall site also contains a Scheduled Ancient Monument.

Proposal

Consent is sought for external and internal repairs and alterations involving replacement of main staircase, installation of lift shaft, reconfiguration of back staircase, removal of entrance porch within central courtyard and construction of glazed roof to central courtyard.

The principal objectives of the work proposed are to: -

- Bring the building into sound repair
- Redecoration of the interior informed by detailed investigation of historic finishes.
- Interventions to improve access, circulation and functionality
- A major upgrade of services

The proposed internal works have been modified since the initial application was made, to incorporate omission of a glazed screen to the lift lobby, following agreement with the Council's Building Control that it was not necessary to provide fire separation in this position.

Relevant planning history

There are a number of planning applications relating to Forty Hall but none relevant to this application.

Consultation

Public

The application was advertised as development affecting a listed building in the Forty Hill Conservation Area.

The Forty Hill and Bulls Cross Study group were consulted and raised the following observations:

- Applicant to record the building, in particular the areas to be altered prior to any works commencing and ideally to an English Heritage approved level.
- Request that applicant has made an adequate assessment of the impact to the surrounding park and has addressed the issues that are likely to occur during and after any works. The Group is particular concerned about parking and access.
- Request that only suitable materials and building methods are used and any new introductions to the building are “ in keeping” for example the operation of the lift should be silent

Internal

Conservation Team

No comments have been received

External

English Heritage

No comments recommend that application should be determined in accordance with national and local policy guidance and on basis of the Council’s specialist conservation advice.

The Victorian Society

Object to the complete demolition of the main staircase, which is a Victorian alteration. Raise the issue that the Design and Access Statement relies upon the Conservation Management Plan (CMP) for its assessment of the significance of the building .The Society contend that this document has led to a highly selective approach to the importance of this Grade 1 listed building which has consequently informed the level of alteration proposed. They state that the overall significance summarised in the CMP excludes any mention of the nineteenth century work in the house, instead emphasis being placed on the eighteenth century fabric.

As a result the Society consider that the proposals attempt to reverse many of the changes of the nineteenth century from circulation routes to decoration on the basis that it is of lesser significance or detrimental impact to the rest of the house. The most significant of these proposed changes being the complete removal of the main staircase, built in 1897.

The Society state that they appreciate the need for disabled access to the upper floors PPG 15 explicitly recommends against the demolition of main staircases in listed buildings. The society acknowledge that although the staircase is not an indication of the date when the house was first constructed it does represent an episode in the building’s development. They support this view

with reference to PPG 15 section 3.13, which states that these cumulative changes are an important reflection of the history of the building's use and ownership.

The Society refers to the Design and Access statement, which indicates that the replacement stair will imitate the form of a pre-Victorian staircase. Although they acknowledge that although there is no evidence of the appearance of the original the Society consider it would be better to adapt the existing structure to accommodate the lift shaft which in their view would achieve a more sustainable solution which retained the historical fabric of the building.

Ancient Monument Society

Recognise that most controversial aspect of the project is the removal of the present stairs and introduction of a lift. Society feel the application is justified and what is supplanted is clearly of secondary interest. Consider project promises to revive the fortunes of the greatest secular property in Enfield and to be applauded.

Council for British Archaeology

The Committee acting on behalf of the Council for British Archaeology discussed the proposal and considered that the proposal to repair and restore the building with the help of Heritage Lottery Funding was welcomed. There has clearly been a careful analysis of the building's history to inform the proposed alterations which aim to make more sense and better use of the building. They had some concern about the loss of the existing stairs that appear to be very much an integral part of the interior, despite being of a later date. On balance, however, the Committee considered that, in principle, the alterations were acceptable subject to detailed design that would need to be set out and agreed with English Heritage.

English Heritage – Archaeology Advisor, (GLAAS)

Reference has been made to the application including information on two separate archaeological interventions, carried out by the Enfield Archaeological Society, in the basement of the present house and against the external north wall. Perhaps the most significant finding is certain proof that there was a substantial building on the site of Forty Hall that predates its 1629 foundation. Walls, flooring and drains associated with this earlier building were seen in the basement, in an area adjacent to the proposed lift pit.

Although it is acknowledged that these investigations have greatly improved understanding of the house's development, and have also reduced the risk of unexpected discoveries, in a place of such significance any impact to the ground or historic fabric has the potential to reveal new information about the site, and to increase our understanding and appreciation of it.

GLAAS do not consider that that any further work need be undertaken prior to determination of the applications for listed building consent and planning permission but that the archaeological position should be reserved by condition.

Reference is made to the document (Forty Hall: Heritage Impact Assessment of Proposed Works) included within the applications that lists the interventions where archaeological monitoring is required. GLAAS consider that this document would probably form the basis of a Written Scheme of Investigation. They would however in addition to the above, recommend expanding archaeological investigations to wherever a new ground impact, such as service arrangements, cabling, or access routes, is required. In this regard they recommend the inclusion of an informative seeking the preparation of an archaeological project design, which should be in accordance with the appropriate English Heritage guidelines.

The Georgian Group

No objections to the proposal

Conservation Advisory Group (CAG)(October 2009)

CAG has been briefed on this project many times before and very happy to see it come forward.

Relevant Policy

The London Plan (2008)

- 4B.1 Design principles for a compact city
- 4B.8 Respect local context and communities
- 4B.11 London' built heritage
- 4B.12 Heritage Conservation
- 4B.13 Historic conservation –led regeneration
- 4B.15 Archaeology

Unitary Development Plan

- (I) GD1: Regard to surroundings
- (I) GD2: Improving the environment
- (II) GD1: Appropriate location for new developments
- (II) GD3: Aesthetics and functional design
- (I) C1: Preserving and enhancing, areas, sites, buildings and landscape features of archaeological, architectural or historic importance.
- (II) C1: To ensure that buildings of architectural or historic interest are preserved or enhanced.
- (II) C12: Managing and maintaining historic buildings.
- (II) C17: To resist substantial built development within historic curtilages
- (II) C19: Historic landscapes
- (II) C20: Management programmes
- (II) C27: To ensure that the setting of buildings of architectural/historic interest is protected.
- (II) C30: New buildings and alterations in the Conservation Area to reflect/complement traditional characteristics of the area.
- (II) G1: To resist inappropriate development in the Green Belt
- (I) G2: Improvement and enhancement of Green Belt.

Other Policy considerations:

Planning Policy Guidance PPG 15: Planning and the Historic Environment.

Planning Policy Guidance PPG16: Archaeology and Planning

Forty Hall and Estate, Enfield- Conservation Management Plan March 2007 (Minor amendments 2008)

Local Development Framework

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and

strategic objectives for the Borough. Extensive consultation on the Core Strategy has taken place and the Proposed Submission Core Strategy document is expected to be out for public consultation before the end of the year. The following Core Strategy policies are of relevance:

SO18: To protect the Borough's conservation areas

CP 23: Built heritage

Analysis

Principle of development

Extensions and alterations to a listed building or within the curtilage of a listed building are acceptable in principle, providing that the proposed development does not affect the character and setting of the listed building or affects the historic fabric of the building.

The proposals for the restoration and refurbishment of Forty Hall are in principle welcomed in terms of both preserving and enhancing the exteriors and interiors of this Grade 1 listed building and providing for greater public use.

Listed building consent is sought for the alterations proposed. This application if deemed to be granted permission will required to be notified by the applicant to the Secretary of State (Government Office for London) under Planning (Listed Buildings and Conservation Areas) Regulations 1990, regulation 13 and the Secretary of State will reconsult English Heritage again on the proposal.

Impact on the listed building

PPG15 recognises that generally the best way of securing the up keep of historic buildings is to keep them in active use.

Issues of general relevance to the consideration of all listed buildings are:

1. The importance of the building, its intrinsic architectural and historic interest and rarity;
2. Physical features of the building
3. The building's setting and contribution to the local scene
4. The benefits to the community derived from the proposals

Generally, the best way to secure the future of historic buildings is to keep them in use and PPG15 acknowledges that to achieve this, buildings may require some degree of adaptation. The proposed repairs and alterations are therefore a material consideration in terms of preserving the special character of the listed building. There being a need to balance the changes proposed against the effect of these changes to the special architectural and historic interest of the building.

The uses of the main building will remain unaltered and no additional floor space is proposed. However, many rooms are not currently open to the public and the second floor is almost entirely disused. The proposed rearrangement of the internal spaces within the hall will release important rooms on the ground and first floor for public use and provide new facilities on the second floor. The percentage of total floor space available for public use will rise from 45% to 78%.

Some concerns have been expressed about the replacement of the main stair with a new stair and lift particularly by the Victorian Society. The decision to site the lift in the centre of the house was informed by an option appraisal produced in January 2009. It was considered that the reconstruction of the central stair would allow the original circulation pattern at first floor level to be reinstated, which it was considered would greatly aid the understanding and legibility of the

historic fabric and circulation through the building. A paper “ Forty Hall: the main stair and lift related to the Conservation principles assessed the new stair against English Heritage’s emerging Conservation Principles and concluded it was in conformity with these principles. English Heritage has been consulted throughout the development of the scheme and has offered no comments on the application as submitted.

Impact on the surrounding park

The Forty Hall and Bulls Cross Study Group raised the issue of the impact on the surrounding park that are likely to occur both during and after the works have been completed in terms particularly of parking and access. The proposed alterations and improvements do not result in a material change of use of the building and therefore there is no presumption that this will result in a change in parking demand .The Conservation Management Plan dated March 2007 considers issues in respect of vehicular access to the park and in particular it acknowledges that the gateway from Forty Hill is relatively narrow and has led to the grade II listed gate piers sustaining frequent damage by large vehicles entering the site. No information on construction traffic has been submitted as part of the application .It is suggested that the applicants should make early contact with the Council’s highway services in respect of this matter.

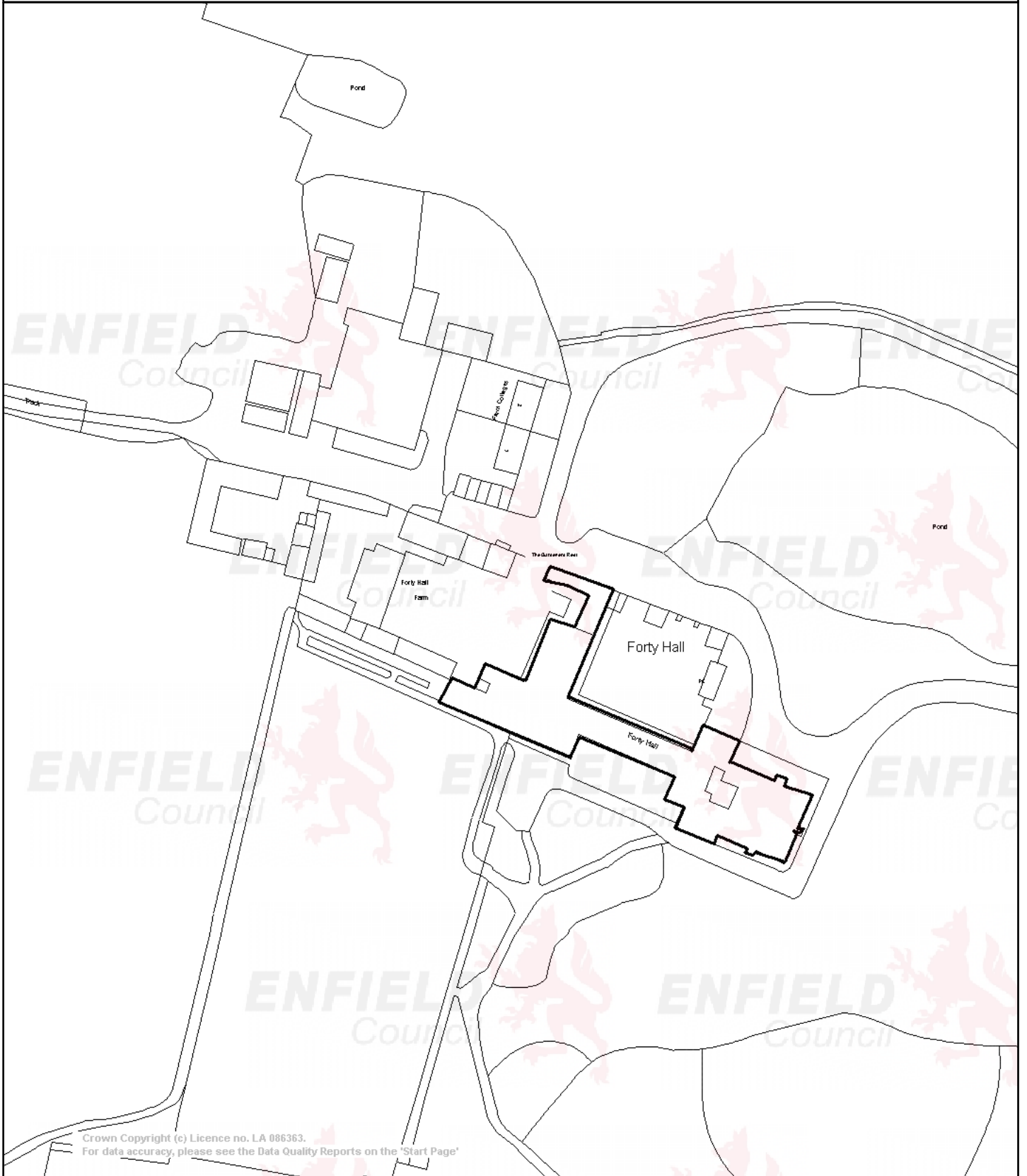
Other matters raised in consultation

The Forty Hill and Bulls Cross Study Group have requested that the applicant records the building prior to any areas being altered and to an English Heritage approved level. The group also raised the issue that suitable materials and building methods are used and for example the operation of the lift should be silent. These details would be addressed by condition.

Conclusion

It is therefore recommended that listed building consent be granted for the following reason.

1. The proposed alterations and repairs to the Grade 1 listed building have appropriate regard to the building’s architectural and historic interest and in this respect comply with Policies (I) C1 and (II) C12 of the Unitary Development Plan.



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Scale 1/1250 Date 1/12/2009

Centre = 533627 E 198581 N

Application Number: LBE/09/0029

Ward: Chase

Date of Registration: 26th August 2009

Contact: Jane Tebbutt 3849

Location: FORTY HALL, FORTY HILL, ENFIELD, EN2 9EU

Proposal: External and internal repairs and alterations involving replacement of main staircase, installation of lift shaft, reconfiguration of back staircase, removal of entrance porch within central courtyard and construction of glazed roof to central courtyard.

Applicant Name & Address:

Liz Cody, London Borough of Enfield, Education, Childrens Services & Leisure
Civic Centre
Silver Street
Enfield
EN1 3XQ

Agent Name & Address:

Mr Brian Lofthouse, Thomas Ford and Partners
177, Kirkdale
Sydenham
SE26 4QH

Recommendation: That planning permission be deemed to be **GRANTED**, subject to Regulation 3 of the Town and Country Planning (General Regulations) 1992, subject to the following conditions: -

1. That development shall not commence until details of the new lift as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The new lift shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

2. That development shall not commence until details of the courtyard roof as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The courtyard roof shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

3. That development shall not commence until details of the replacement staircase as shown on the approved drawings have been submitted to and approved in writing by the Local Planning Authority. The new staircase shall be installed in accordance with the approved details.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the listed building.

4. All new works and works of repair and reinstatement shall match exactly the existing adjacent work with regard to the methods used and to material, colour, texture, profile and

workmanship and in the case of brickwork, facebond and pointing, unless shown otherwise on the drawing or other documentation hereby approved or required by a condition attached to this consent.

Reason: To ensure a satisfactory appearance to the development and to preserve the special character and appearance of the Listed Building.

5. No development shall take place until the applicant has secured the implementation of a programme of archaeological work, in accordance with a written scheme of investigation which has been submitted by the applicant and approved by the Local Planning Authority.

Reason: To safeguard archaeological heritage.

6. That development shall not commence until the applicant has recorded to English Heritage standards all areas of the building to be repaired or altered. The scheme of recording to be submitted to and approved by the Local Planning Authority.

Reason: To ensure that a full record is retained of the building's historical heritage.

7. Development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:

- Details of construction access and vehicle routing to the site
- Arrangements for vehicle turning and servicing areas
- Arrangements for the parking of contractors' vehicles
- Arrangements for the storage of materials

Reason: In order to protect the amenities of nearby residential properties, to ensure access does not prejudice the free flow and safety of traffic and pedestrians along the adjoining highways.

8. C22 Details of Const. Vehicle Wheel Cleaning

9. C51A Time Limited Permission

Site and surrounding

Forty Hall is a grade I listed building situated within Forty Hill and Bulls Cross Conservation Area, the Green Belt and Area of Special Character. It is also included on the National Register of Historic Parks and Gardens. The Forty Hall site also contains a Scheduled Ancient Monument.

Proposal

Consent is sought for external and internal repairs and alterations involving replacement of main staircase, installation of lift shaft, reconfiguration of back staircase, removal of entrance porch within central courtyard and construction of glazed roof to central courtyard.

The principal objectives of the work proposed are to: -

- Bring the building into sound repair
- Redecoration of the interior informed by detailed investigation of historic finishes.
- Interventions to improve access, circulation and functionality
- A major upgrade of services

The proposed internal works have been modified since the initial application was made, to incorporate omission of a glazed screen to the lift lobby, following agreement with the Council's Building Control that it was not necessary to provide fire separation in this position.

Relevant planning history

There are a number of planning applications relating to Forty Hall but none relevant to this application.

Consultation

Public

The application was advertised as development affecting a listed building in the Forty Hill Conservation Area.

The Forty Hill and Bulls Cross Study group were consulted and raised the following observations:

- Applicant to record the building, in particular the areas to be altered prior to any works commencing and ideally to an English Heritage approved level.
- Request that applicant has made an adequate assessment of the impact to the surrounding park and has addressed the issues that are likely to occur during and after any works. The Group is particular concerned about parking and access.
- Request that only suitable materials and building methods are used and any new introductions to the building are "in keeping" for example the operation of the lift should be silent

Internal

Conservation Team

No comments have been received.

External

English Heritage

No comments recommend that application should be determined in accordance with national and local policy guidance and on basis of the Council's specialist conservation advice.

The Victorian Society

Object to the complete demolition of the main staircase, which is a Victorian alteration. Raise the issue that the Design and Access Statement relies upon the Conservation Management Plan (CMP) for its assessment of the significance of the building .The Society contend that this document has led to a highly selective approach to the importance of this Grade 1 listed building which has consequently informed the level of alteration proposed. They state that the overall significance summarised in the CMP excludes any mention of the nineteenth century work in the house, instead emphasis being placed on the eighteenth century fabric.

As a result the Society consider that the proposals attempt to reverse many of the changes of the nineteenth century from circulation routes to decoration on the basis that it is of lesser significance or detrimental impact to the rest of the house. The most significant of these proposed changes being the complete removal of the main staircase, built in 1897.

The Society state that they appreciate the need for disabled access to the upper floors PPG 15 explicitly recommends against the demolition of main staircases in listed buildings. The society acknowledge that although the staircase is not an indication of the date when the house was first constructed it does represent an episode in the building's development. They support this view with reference to PPG 15 section 3.13, which states that these cumulative changes are an important reflection of the history of the building's use and ownership.

The Society refers to the Design and Access statement, which indicates that the replacement stair will imitate the form of a pre-Victorian staircase. Although they acknowledge that although there is no evidence of the appearance of the original the Society consider it would be better to adapt the existing structure to accommodate the lift shaft which in their view would achieve a more sustainable solution which retained the historical fabric of the building.

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Recognise that most controversial aspect of the project is the removal of the present stairs and introduction of a lift. Society feel the application is justified and what is supplanted is clearly of secondary interest. Consider project promises to revive the fortunes of the greatest secular property in Enfield and to be applauded.

Council for British Archaeology

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English Heritage – Archaeology Advisor, (GLAAS)

Reference has been made to the application including information on two separate archaeological interventions, carried out by the Enfield Archaeological Society, in the basement of the present house and against the external north wall. Perhaps the most significant finding is certain proof that there was a substantial building on the site of Forty Hall that predates its 1629 foundation. Walls, flooring and drains associated with this earlier building were seen in the basement, in an area adjacent to the proposed lift pit.

Although it is acknowledged that these investigations have greatly improved understanding of the house's development, and have also reduced the risk of unexpected discoveries, in a place of such significance any impact to the ground or historic fabric has the potential to reveal new information about the site, and to increase our understanding and appreciation of it.

GLAAS do not consider that that any further work need be undertaken prior to determination of the applications for listed building consent and planning permission but that the archaeological position should be reserved by condition.

Reference is made to the document (Forty Hall: Heritage Impact Assessment of Proposed Works) included within the applications that lists the interventions where archaeological monitoring is required. GLAAS consider that this document would probably form the basis of a Written Scheme of Investigation. They would however in addition to the above, recommend expanding archaeological investigations to wherever a new ground impact, such as service arrangements, cabling, or access routes, is required. In this regard they recommend the inclusion of an informative seeking the preparation of an archaeological project design, which should be in accordance with the appropriate English Heritage guidelines.

The Georgian Group

No objections to the proposal

Conservation Advisory Group (CAG)(October 2009)

CAG has been briefed on this project many times before and very happy to see it come forward.

Relevant Policy

The London Plan (2008)

- 4B.1 Design principles for a compact city
- 4B.8 Respect local context and communities
- 4B.11 London' built heritage
- 4B.12 Heritage Conservation
- 4B.13 Historic conservation –led regeneration
- 4B.15 Archaeology

Unitary Development Plan

- (I) GD1: Regard to surroundings
- (I) GD2: Improving the environment
- (II) GD1: Appropriate location for new developments
- (II) GD3: Aesthetics and functional design
- (II) GD8: Site access and servicing
- (I) C1: Preserving and enhancing, areas, sites, buildings and landscape features of archaeological, architectural or historic importance.
- (II) C1: To ensure that buildings of architectural or historic interest are preserved or enhanced.
- (II) C12: Managing and maintaining historic buildings.
- (II) C17: To resist substantial built development within historic curtilages
- (II) C19: Historic landscapes
- (II) C20: Management programmes
- (II) C27: To ensure that the setting of buildings of architectural/historic interest is protected.
- (II) C30: New buildings and alterations in the Conservation Area to reflect/complement traditional characteristics of the area.
- (II) G1: To resist inappropriate development in the Green Belt
- (I) G2: Improvement and enhancement of Green Belt.

Other Policy considerations:

Planning Policy Guidance PPG 15: Planning and the Historic Environment.
Planning Policy Guidance PPG16: Archaeology and Planning

Forty Hall and Estate, Enfield- Conservation Management Plan March 2007 (Minor amendments 2008)

Local Development Framework

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. Extensive consultation on the Core Strategy has taken place and the Proposed Submission Core Strategy Document is expected to be out for public consultation before the end of the year. The following Core Strategy policies are of relevance:

SO18: To protect the Borough's conservation areas

CP 23: Built heritage

Analysis

Principle of development

The proposals for the restoration and refurbishment of Forty Hall are in principle welcomed in terms of both preserving and enhancing the exteriors and interiors of this Grade 1 listed building and providing for greater public use.

Planning permission and listed building consent is sought for the alterations proposed.

Impact on the listed building

PPG15 recognises that generally the best way of securing the up keep of historic buildings is to keep them in active use.

Issues of general relevance to the consideration of all listed buildings are:

1. The importance of the building, its intrinsic architectural and historic interest and rarity;
2. Physical features of the building
3. The building's setting and contribution to the local scene
4. The benefits to the community derived from the proposals

Generally, the best way to secure the future of historic buildings is to keep them in use and PPG15 acknowledges that to achieve this, buildings may require some degree of adaptation. The proposed repairs and alterations are therefore a material consideration in terms of preserving the special character of the listed building. There being a need to balance the changes proposed against the effect of these changes to the special architectural and historic interest of the building.

The uses of the main building will remain unaltered and no additional floor space is proposed. However, many rooms are not currently open to the public and the second floor is almost entirely disused. The proposed rearrangement of the internal spaces within the hall will release important rooms on the ground and first floor for public use and provide new facilities on the second floor. The percentage of total floor space available for public use will rise from 45% to 78%.

Some concerns have been expressed about the replacement of the main stair with a new stair and lift particularly by the Victorian Society. The decision to site the lift in the centre of the house was informed by an option appraisal produced in January 2009. It was considered that the reconstruction of the central stair would allow the original circulation pattern at first floor level to be reinstated, which it was considered would greatly aid the understanding and legibility of the

historic fabric and circulation through the building. A paper “ Forty Hall: the main stair and lift related to the Conservation principles assessed the new stair against English Heritage’s emerging Conservation Principles and concluded it was in conformity with these principles. English Heritage has been consulted throughout the development of the scheme and has offered no comments on the application as submitted.

Impact on the surrounding park

The Forty Hall and Bulls Cross Study Group raised the issue of the impact on the surrounding park that are likely to occur both during and after the works have been completed in terms particularly of parking and access. The proposed alterations and improvements do not result in a material change of use of the building and therefore there is no presumption that this will result in a change in parking demand .The Conservation Management Plan dated March 2007 considers issues in respect of vehicular access to the park and in particular it acknowledges that the gateway from Forty Hill is relatively narrow and has led to the grade II listed gate piers sustaining frequent damage by large vehicles entering the site. No information on construction traffic has been submitted as part of the application .It is suggested that the applicants should make early contact with the Council’s highway services in respect of this matter.

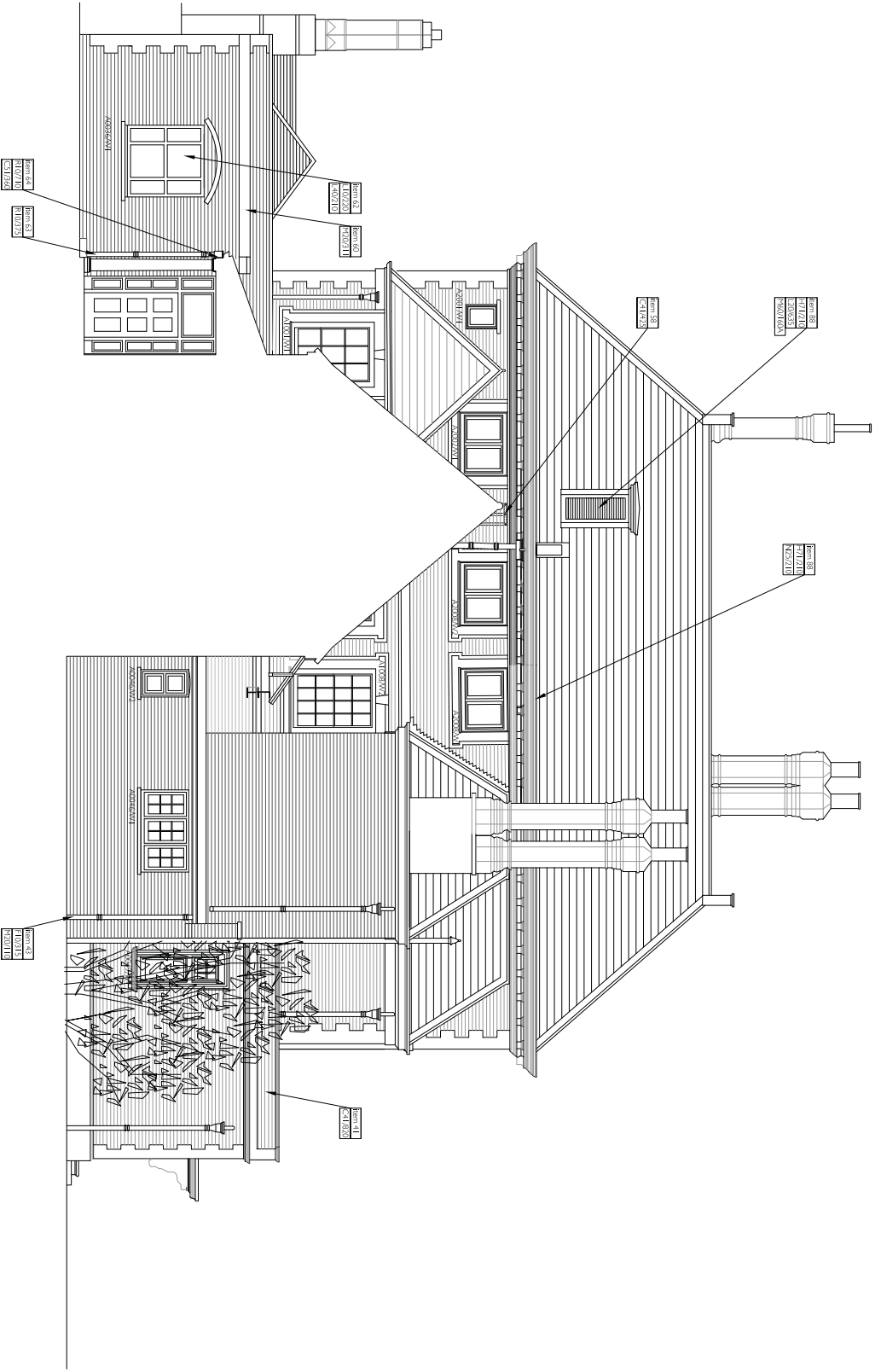
Conclusion

It is therefore recommended that planning permission be granted for the following reason.

1.The proposed alterations and repairs to the Grade 1 listed building have appropriate regard to the building’s architectural and historic interest and in this respect comply with Policies (I) C1 and (II) C12 of the Unitary Development Plan.

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DESIGNER TO BE REFERRED TO THE ARCHITECT.

FORM 1	SCHEDULE OF WORKS/ITEM REF.
FORM 2	MS WORKS SPECIFICATION REF.
FORM 3	LOCATION NUMBER



LBC APPLICATION	
C	LBC application
14/08/20	
STAGE D ISSUE	
B	Single Draw
A	Items 11, 13, 62, 62 and 64
	drawn into Section.
REV.	DATE
APP. NUMBER	DATE

THOMAS FORD & PARTNERS
 CHARTERED ARCHITECTS SURVEYORS
 177 BRIDGEMERE
 STONEMAN • LONDON • SE16 4QH
 TEL: 020 7466 3230 • FAX: 020 7897 3146
 EMAIL: info@thomasford.co.uk

PROJECT			
ENFIELD FORTY HALL HLF STAGE 2			
TITLE			
WEST FLANK ELEVATIONS EXTERNAL REPAIRS			
DRAWN	DATE	SCALE	CHECKED
20/03/09	18/06/04		
PROJECT NO.	DRAWING NO.	REV.	
841192	04/21	C	

0 5 10 METRES

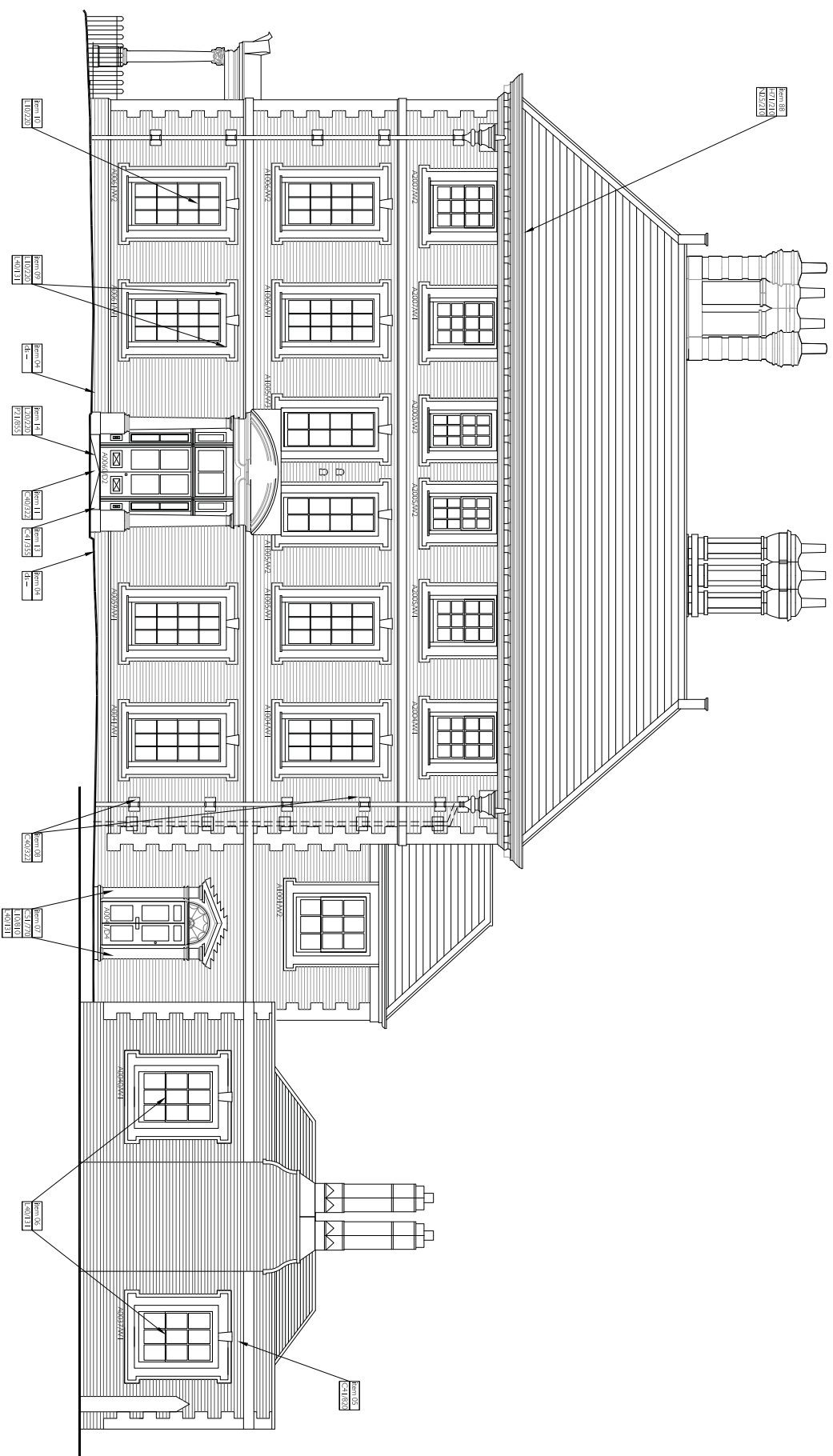
SCALE 1:50 (A1)

SCALE 1:100 (A3)

DO NOT SCALE DIMENSIONS, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND DECISIONS TO BE REFERRED TO THE ARCHITECT.

REF	SCHEDULE OF WORKS/SECTION REF.
1	MS WORKS SPECIFICATION REF.
2	
3	

LOCATION: 19/01/04



0 METRES 5 METRES 10 METRES

SCALE 1:50 (A1)
SCALE 1:100 (A3)

LBC APPLICATION	
D	LBC application
C	Workshop provided to Stage 02 for 10 days.
STAGE D ISSUE	
B	Single Issue
A	Items 07 and 14 above ref. re-issued.
REV	DATE

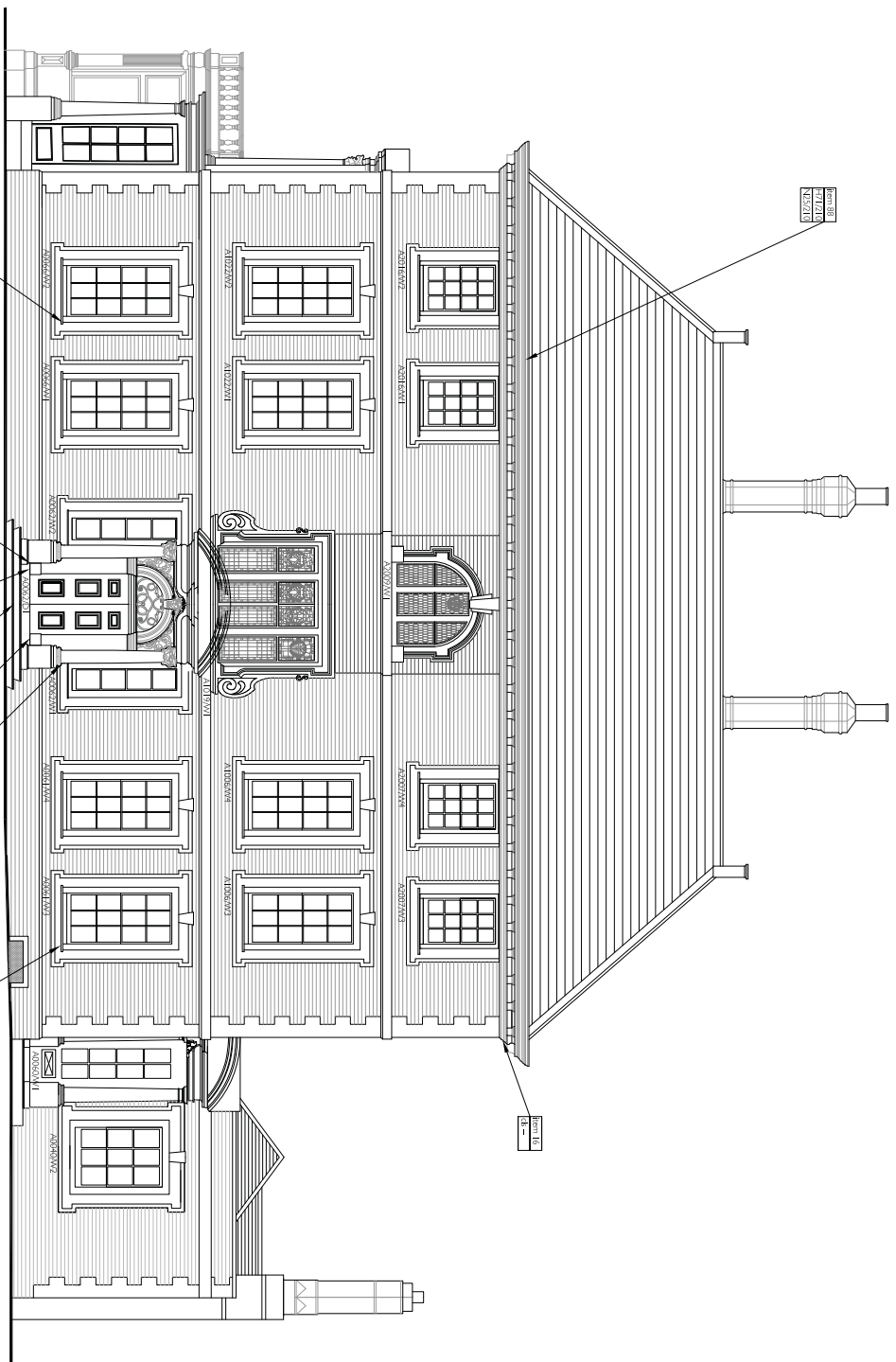
THOMAS FORD & PARTNERS
CHARTERED ARCHITECTS SURVEYORS
 177 BRIDGEMAN
 STONEMAN • LONDON • SE16 4QH
 TEL: 020 8446 3230 • FAX: 020 8937 3146
 E-MAIL: info@thomasford.com

PROJECT			
ENFIELD FORTY HALL			
HLF STAGE 2			
TITLE			
NORTH ELEVATION			
EXTERNAL REPAIRS			
DRAWN	DATE	SCALE	CHECKED
	20.03.09	1:50 (A1)	
PROJECT NO.	REV	DATE	BY
841192	04/20		

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE FOR THE ACCURACY OF DIMENSIONS TO BE REFERRED TO THE ARCHITECT.

Room	SCHEDULE OF WORKS/SECTION REF.
11	MS WORKS SPECIFICATION REF.
12	
13	

LOCATION NUMBER:



0 METRES 5 METRES 10 METRES

SCALE 1:50 (A1)
SCALE 1:100 (A3)

LBC APPLICATION	
C LBC application	14/08/09
STAGE D ISSUE	
B Stage D Issue	15/08/09
A Items 19 and 22 have refs. rechecked.	08/08/09
REV	DATE

THOMAS FORD & PARTNERS
CHARTERED ARCHITECTS SURVEYORS

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E-MAIL: info@thomasford.co.uk

PROJECT
ENFIELD
FORTY HALL
HLF STAGE 2

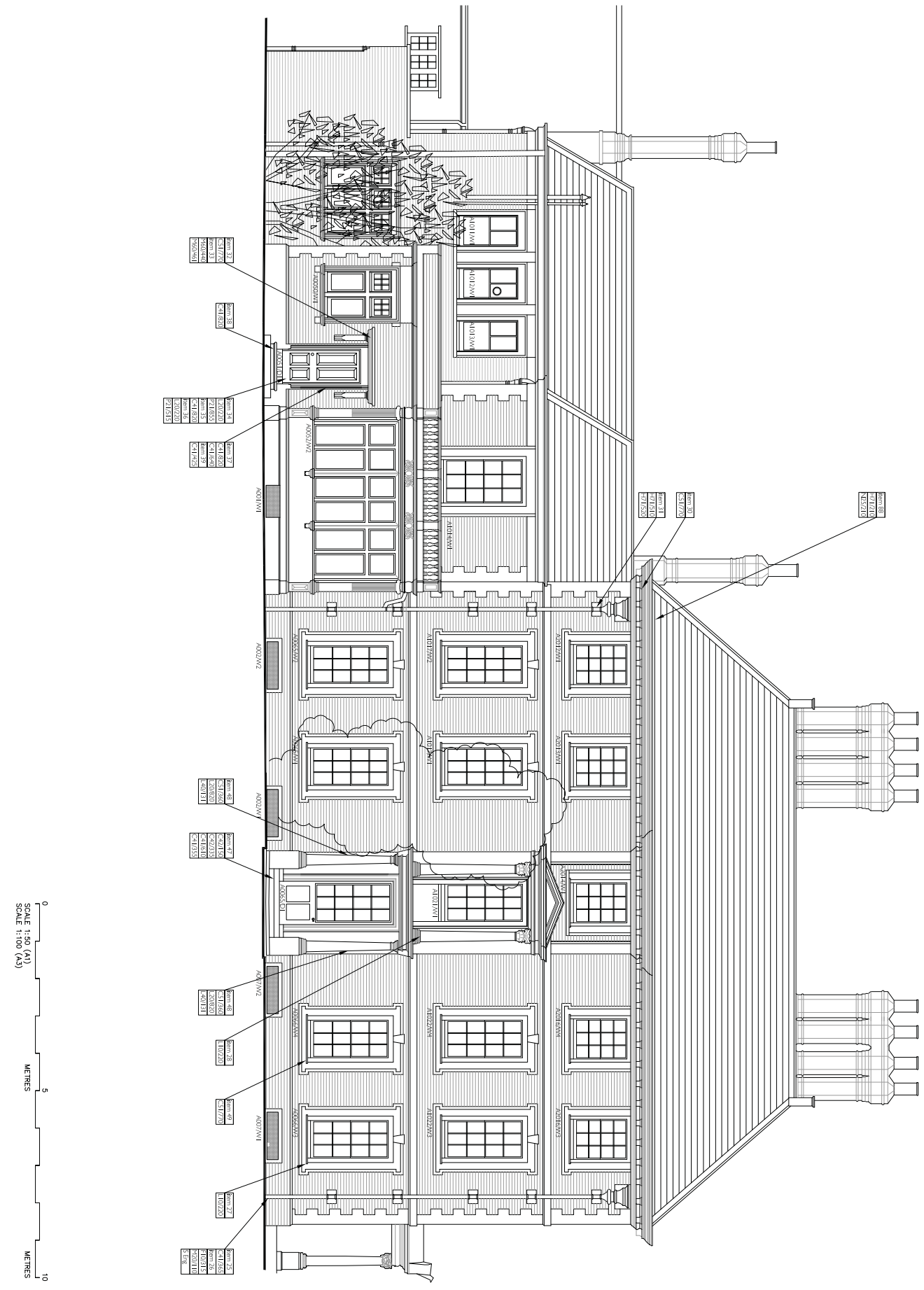
DATE	SCALE	CREATED
20/03/09	1:50 (A1)	
Rev 1		
Rev 2		
Rev 3		
Rev 4		
Rev 5		
Rev 6		
Rev 7		
Rev 8		
Rev 9		
Rev 10		
Rev 11		
Rev 12		
Rev 13		
Rev 14		
Rev 15		
Rev 16		
Rev 17		
Rev 18		
Rev 19		
Rev 20		
Rev 21		
Rev 22		
Rev 23		
Rev 24		

TITLE
EAST ELEVATION
EXTERNAL REPAIRS

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DESIGNER SHALL BE REFERRED TO THE ARCH-TEXT.

REF	SCHEDULE OF WORKS/SECTION REF.
1	MS WORKS SPECIFICATION REF.
2	
3	

LOCATION: 09/09/04



LBC APPLICATION		1-4/08/09
C LBC application		
STAGE D ISSUE		
B	Single Drawn	1-5/08-09
A	Items 26, 36, 34, 36, 47 and 49	08/08-09
	drawn into section.	
REV	APPREWER	DATE

THOMAS FORD & PARTNERS
 CHARTERED ARCHITECTS
 SURVEYORS

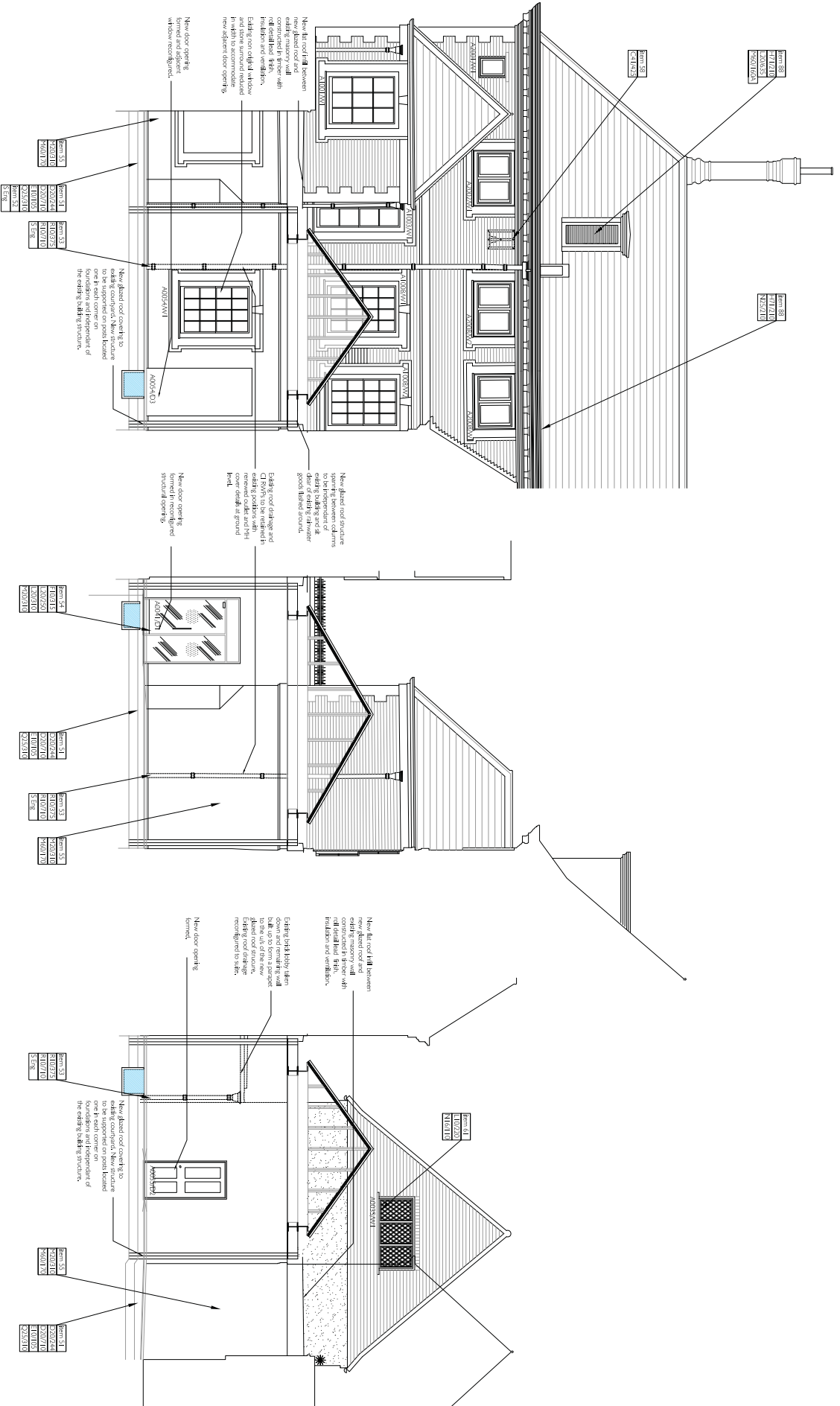
177 BRIDGDALE
 STONINGHAM • COBOROON • SE26 4QH
 TEL: 01703 63200 • FAX: 01703 6391346
 E-MAIL: info@thomasford.co.uk

PROJECT			
ENFIELD FORTY HALL			
HLF STAGE 2			
TITLE			
SOUTH ELEVATION			
EXTERNAL REPAIRS			
DRAWN	DATE	SCALE	CHECKED
20/03/09	1:50 (A1)		
PROJECT NO:	REV	BY	
8411922	04/18	C	

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE FOR THE ACCURACY OF DIMENSIONS TO BE REFERRED TO THE ARCHITECT.

REV	DESCRIPTION	SCHEDULE OF WORK/SECTION REF.	NBS WORKS SPECIFICATION REF.
1			
2			
3			

LOCATION: 169/000



0 5 10 METRES
SCALE 1:50 (A1)
SCALE 1:100 (A3)

NO.	REV.	DATE
	REVISED	
<p>THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS</p> <p>177 BRIDDALE STONEMAN • LONDON • SE8 4QH TEL: 020 7616 3230 • FAX: 020 7616 3146 E: info@thomasford.com</p>		
<p>COURTYARD ENFIELD FORTY HALL HL/STAGE 2</p> <p>AS PROPOSED</p>		
PROJECT	SCALE	CHECKED
06.01.09	1:50 (A1)	REV
841192	CA/1	E

<p>THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS</p> <p>177 BRIDDALE STONEMAN • LONDON • SE8 4QH TEL: 020 7616 3230 • FAX: 020 7616 3146 E: info@thomasford.com</p>		
<p>COURTYARD ENFIELD FORTY HALL HL/STAGE 2</p> <p>AS PROPOSED</p>		
PROJECT	SCALE	CHECKED
06.01.09	1:50 (A1)	REV
841192	CA/1	E

DO NOT SCALE DRAWINGS, THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE TO THE ARCHITECT.

REV	SCHEDULE OF WORK/REVISION REF.
1	MIS WORKS SPECIFICATION REF.
2	
3	

LOCATION: N990004

H	APPROVED BY ARCHITECT DATE: 18.08.09
LBC APPLICATION	
G	LBC application 14.08.09
F	HAB suit reconfigured to suit 11.07.09
MIS WORKS SPECIFICATION REF.	
STAGE D ISSUE	
E	Sage Office 15.05.09
D	HAB 56 Chase F10 red reworked, 06.05.09
C	As per above, but door reworked to suit, 11.04.09
B	Door and window numbering added as room data sheet ref., 21.03.09
A	Additional detail of courtyard glasser roof configuration added, 09.03.09
REV	DATE
REV	DATE

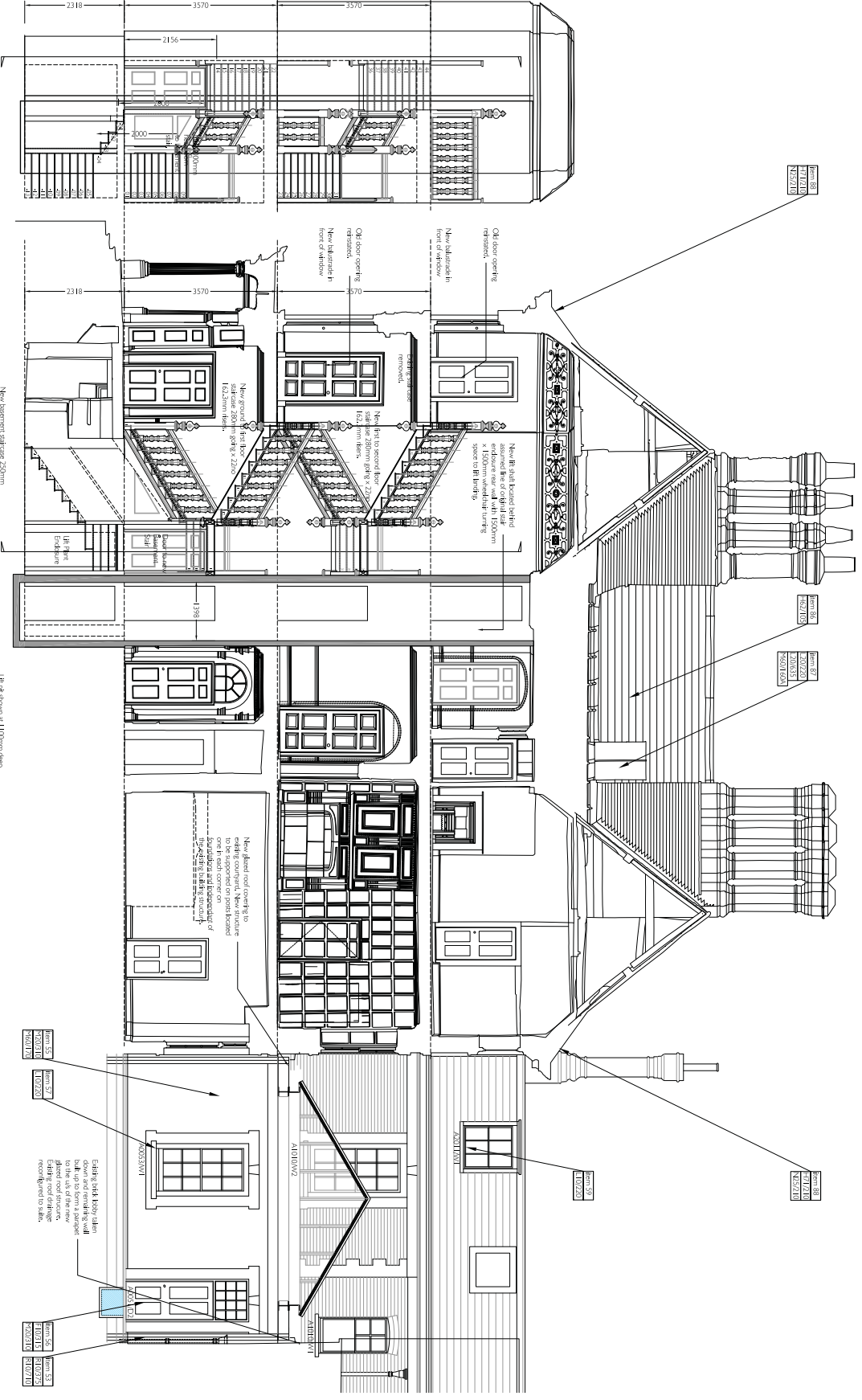
THOMAS FORD & PARTNERS CHARTERED ARCHITECTS ARCHITECTS

177 BRICKDALE STONEYBAY • COBORN • SEAS COEH
TEL: 007 607 5235 • FAX: 007 608 973 146
E: thomas.ford@thomasford.co.uk

PROJECT
ENFIELD FORTY HALL
HLF STAGE 2

SECTIONAL ELEVATION
PROPOSED

DRAWN	DATE	SCALE	CHECKED
06.01.09	15.05.09	1:50 (A1)	
REV. NO.	REV. BY	REV. DATE	REV. BY
04/1302	CAL		HL



New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

Old door opening reworked front of window.

New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

Old door opening reworked front of window.

New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

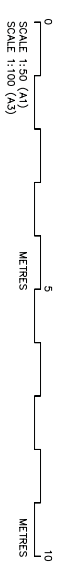
Old door opening reworked front of window.

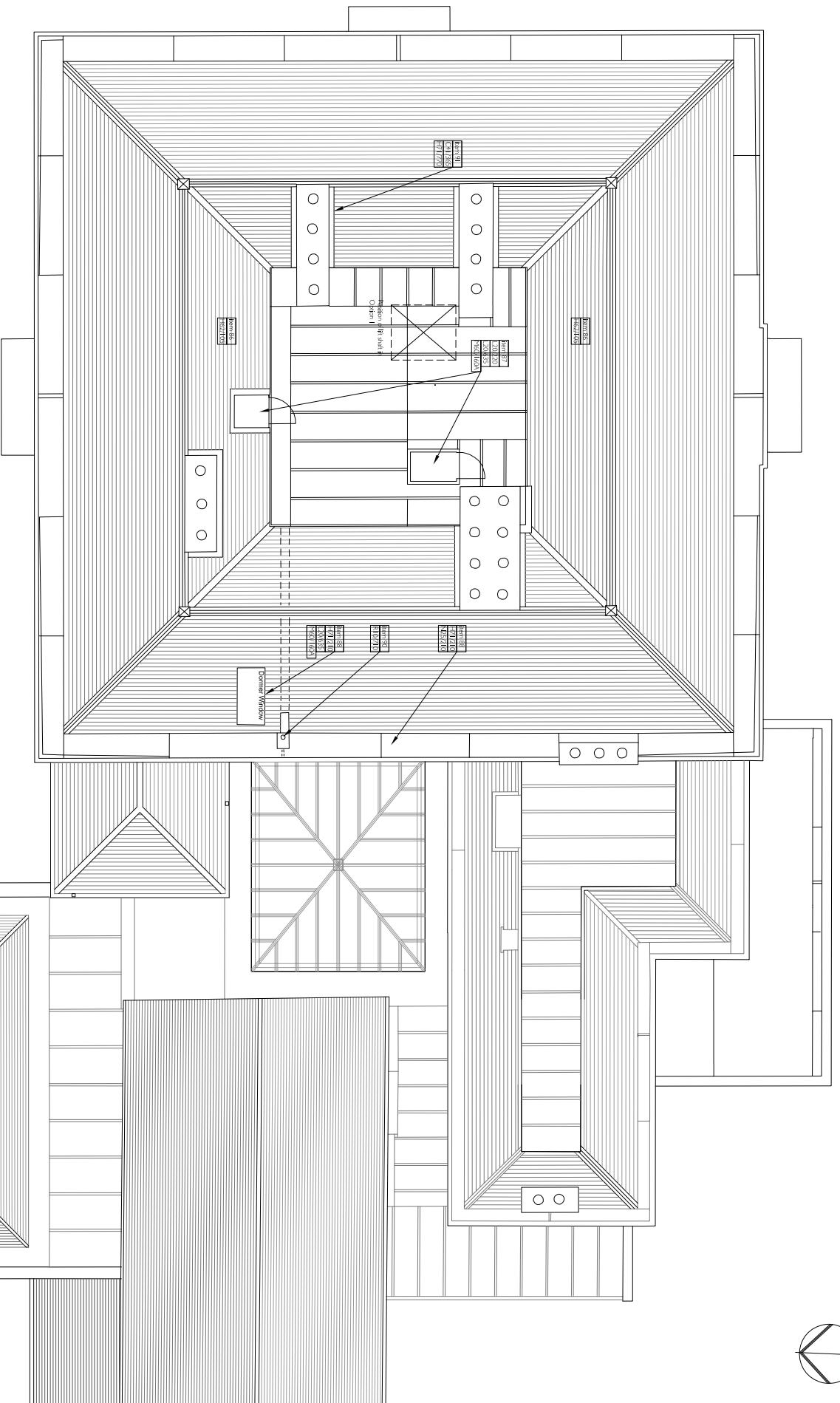
New fire suit based surface: 280mm girth x 22no 162.5mm ribs.

Left of above at 1100mm deep to base on the wall for fitting basement floor level. Refer to original structural drawings for details of concrete and steel to be removed to accommodate duct and rail.

New glass roof covering to existing courtyard. New structure to be supported on post located on existing slab. Refer to structural drawings and schedule of materials for further details of proposed glass roof structure. Existing roof structure to be retained in situ.

Existing brick lobby taken down and reworking wall to the left of the new glass roof structure. Existing roof structure to be retained in situ.





0
SCALE 1:50 (A1)
SCALE 1:100 (A2)

METRES

5

METRES

10

DO NOT SCALE DRAWINGS. THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE FOR THE ACCURACY OF ALL DIMENSIONS TO BE REFERRED TO THE ARCHITECT.

LBC APPLICATION	
B LBC application	14.08.09
STAGE D ISSUE	
A Stage Draw	15.05.09
REV.	DATE

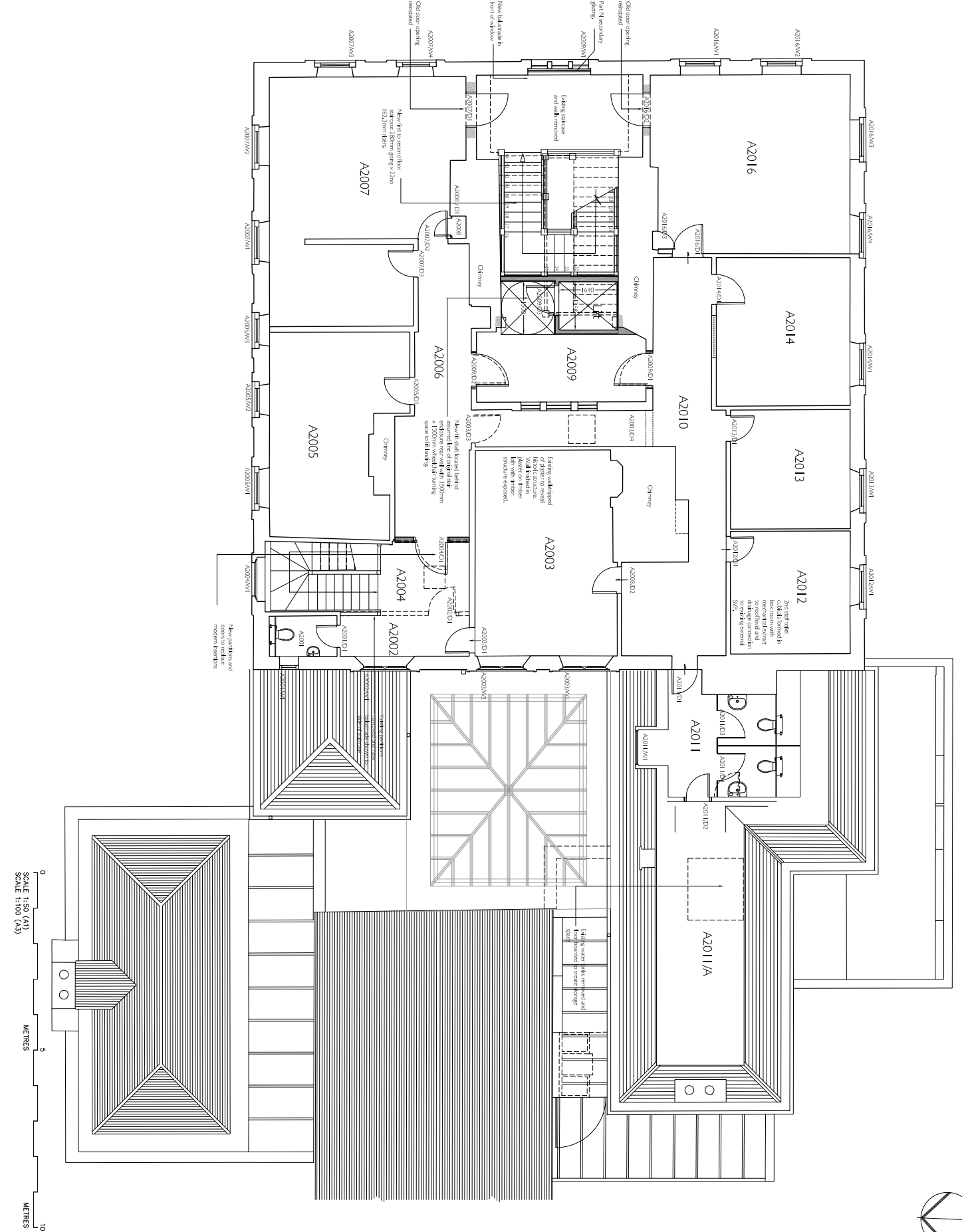
THOMAS FORD & PARTNERS
CHARTERED ARCHITECTS SURVEYORS

177 BRIDGEMERE
STONEMAN • LONDON • SE16 4QH
TEL: 020 7626 3230 • FAX: 020 7697 9146
EMAIL: info@thomasford.co.uk

PROJECT
ENFIELD
FORTY HALL
HLE STAGE 2

TITLE
ROOF PLAN
PROPOSED

DRAWN	DATE	SCALE	CHECKED
06.01.09	1:50 (A1)		
84.11.02			



0 5 10 METRES
 SCALE 1:50 (A1)
 SCALE 1:100 (A3)

DO NOT SCALE DRAWINGS. THE CONTRACTOR AND ARCHITECT TO BE REFERRED TO THE ARCHITECT.

LBC APPLICATION

- I LBC application 14/08/09
- H High salt reconfigured to suit 11/07/09
- I New wall to A2009 and A2010 06/05/09
- G Tidal and female WC partitions removed from A2011 30/04/09
- G partitions to A2003 retained. 30/04/09

STAGE D ISSUE

- F Stage D issue 11/05/09
- F Existing wall to be removed and new partition to be constructed 06/05/09
- E Floor slab reconfigured to suit meeting with RO and SEC 06/05/09
- D New wall to A2009 and A2010 11/04/09
- C New wall to A2009 and A2010 11/04/09
- B Door and window removal and door fitted into A2010. 11/01/09
- A Additional detail and dimensions for wall slab and fit confirmation. 11/01/09

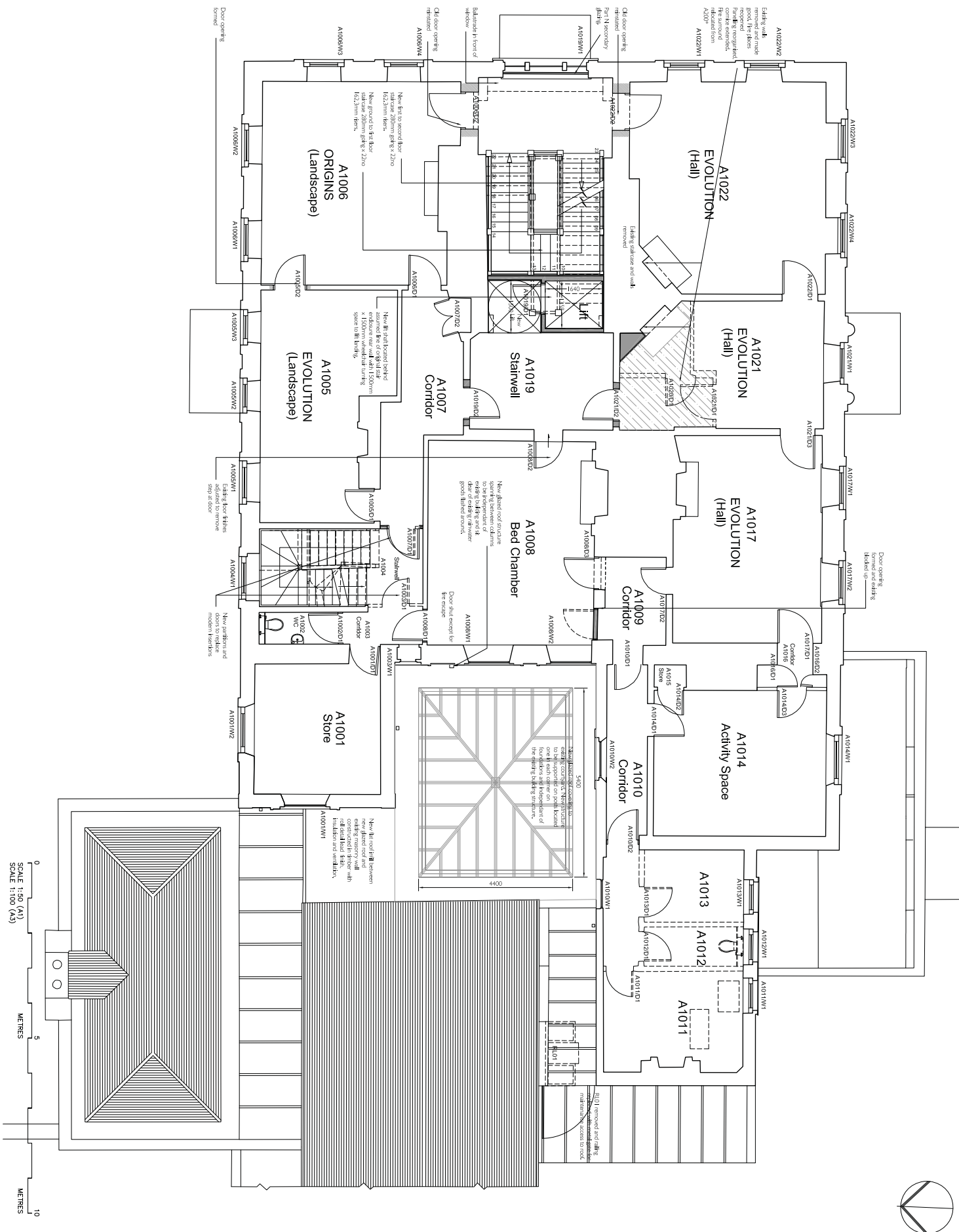
THOMAS FORD & PARTNERS
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 STONINGHAM • LONDON • SE16 4QH
 TEL: 020 7463 3238 • FAX: 020 8979 3146
 E-MAIL: info@thomasford.co.uk

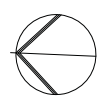
PROJECT
 ENFIELD FORTY HALL
 HLF STAGE 2

TITLE
 SECOND FLOOR PLAN
 PROPOSED

DESIGNED	DATE	SCALE	CHECKED
06/01/09	1:50 (A1)		
8/11/02	1:100 (A3)		



DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DESIGNER IS TO BE REFERRED TO THE ARCHITECT.



LBC APPLICATION

L	LBC application	14/08/09
K	Drawn A1001.DWG, A1021.DWG, A1022.DWG from room plans and utility reworked to suit existing.	12/08/09
J	High level reconfigured to suit existing. Room still reworked. A1002.DWG new.	11/07/09
I	Rooms A1011, A1012 and A1013 completed.	30/06/09
H	Rooms A1011, A1012 and A1013 completed.	28/05/09

STAGE D ISSUE

G	Stage D Issue	15/05/09
F	Fire door problem resolved after meeting with CO and BC.	08/05/09
E	New main stair layout revised to 11.04.09 for delivery to the contractor.	11.04.09
D	Rooms A1011 to A1013 completed.	06/04/09
C	Door and window numbering, adding guidelines and new doors added to room data sheet refile.	11.01.09
B	Additional detail of courtyard glass roof configuration added.	09/01/09
A	Additional detail and specifications for the glass roof structure. The main stair and fire configurations added.	17/12/08

THOMAS FORD & PARTNERS
 CHARTERED ARCHITECTS & SURVEYORS

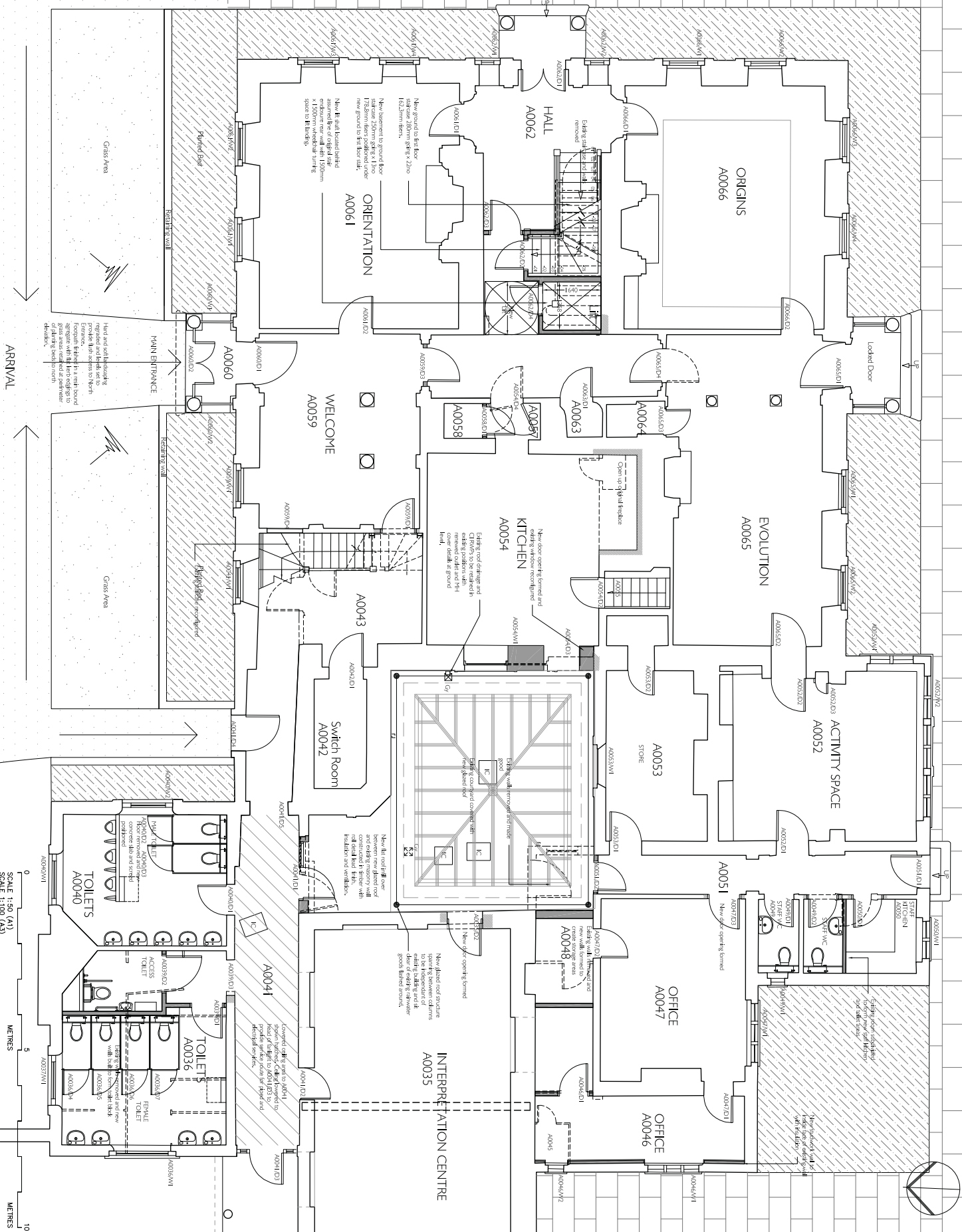
177 BRIDGDALE
 STONINGHAM • LONDON • SE16 4QH
 TEL: 020 7891 3230 • FAX: 020 7891 3146
 EMAIL: info@thomasford.co.uk

PROJECT
 ENFIELD FORTY HALL
 HLF STAGE 2

TITLE
 FIRST FLOOR PLAN
 PROPOSED

ISSUED	DATE	SCALE	CREATED
06.01.09	15/05/09	As Shown	TF
04.11.02	04.11		TF

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DESIGNER IS TO BE REFERRED TO THE ARCHITECT.



SCALE 1:50 (A1)
SCALE 1:50 (A3)

0 5 10 METRES

DESIGNER	DATE	SCALE	CHECKED
THOMAS FORD & PARTNERS ARCHITECTS CHARTERED SURVEYORS	06.01.09	1:50 (A1)	K
PROJECT	CHECKED	CAD	REV
ENFIELD FORTY HALL HE STAGE 2	1:50 (A1)	06.17.09	K
TITLE			
GROUND FLOOR PLAN PROPOSED			
STORAGE + CORNER + SENS. RCH 177 BRIDLE TEL: 02045 2336 • FAX: 02045 991316 EMAIL: info@thomasford.co.uk			

- | NO. | AMENDMENT | DATE |
|----------------------|---|----------|
| STAGE D ISSUE | | |
| A | Applied detail and operations to windows, doors, lifts and lifts shafts and lift configurations | 17.12.09 |
| B | New floor carpet to be formed and ceiling to be redecorated | 09.12.09 |
| C | Door and window numbering | 11.01.09 |
| D | Added room data sheet title | 09.12.09 |
| E | Spec use designation to cover 06.04.09 | |
| F | The door prober to be added | 08.05.09 |
| G | Single D Issue | 15.05.09 |
| H | Revised floor levels and levels | 11.04.09 |
| I | Labeling of rooms added and removed | 12.08.09 |
| J | UIC application | 14.08.09 |
| K | Revised floor levels and levels | 11.07.09 |

UIC application

14.08.09

Labeling of rooms added and removed

12.08.09

Revised floor levels and levels

11.07.09

Revised floor levels and levels

11.04.09

The door prober to be added

08.05.09

Single D Issue

15.05.09

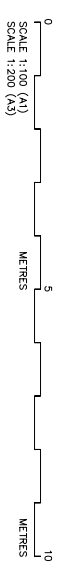
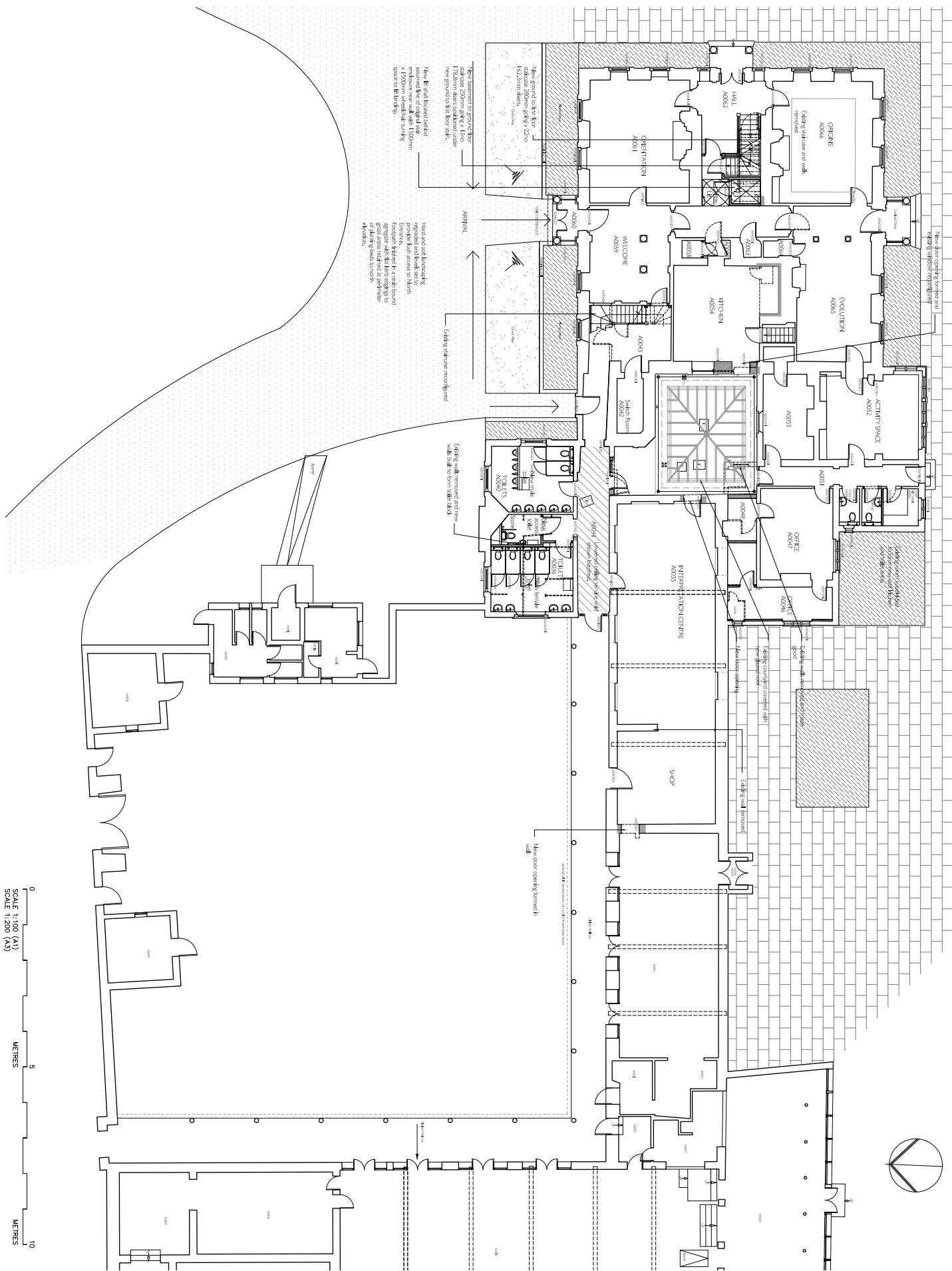
Applied detail and operations to windows, doors, lifts and lifts shafts and lift configurations

17.12.09

New floor carpet to be formed and ceiling to be redecorated

09.12.09

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DIMENSIONS TO BE REFERRED TO THE ARCHITECT.



1	Reinforcing walls to grass areas Address	16.08.07
LBC APPLICATION		
H	LBC application	14.08.07
G	Thin slab reconfigured to suit historic entrance lobby change above retained in A23	11.07.07
STAGE D ISSUE		
F	Stage D Issue	15.05.07
E	Fire door problem resolved after meeting with FD and SAC	08.05.07
D	New main staff layout related to the deskwork fit completed	11.04.07
C	Shop details in indicated from completed to 24.03.07 sent for the DOE and window numbering	06.04.07
B	Door and window numbering added as room data sheet info.	21.03.07
A	Finalised spatial and structural requirements for the proposed revised floor plan and B configuration main staff and B configuration	17.02.07
REV	APPREWER	DATE

THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS

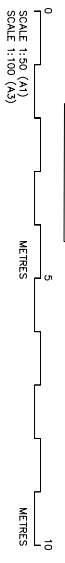
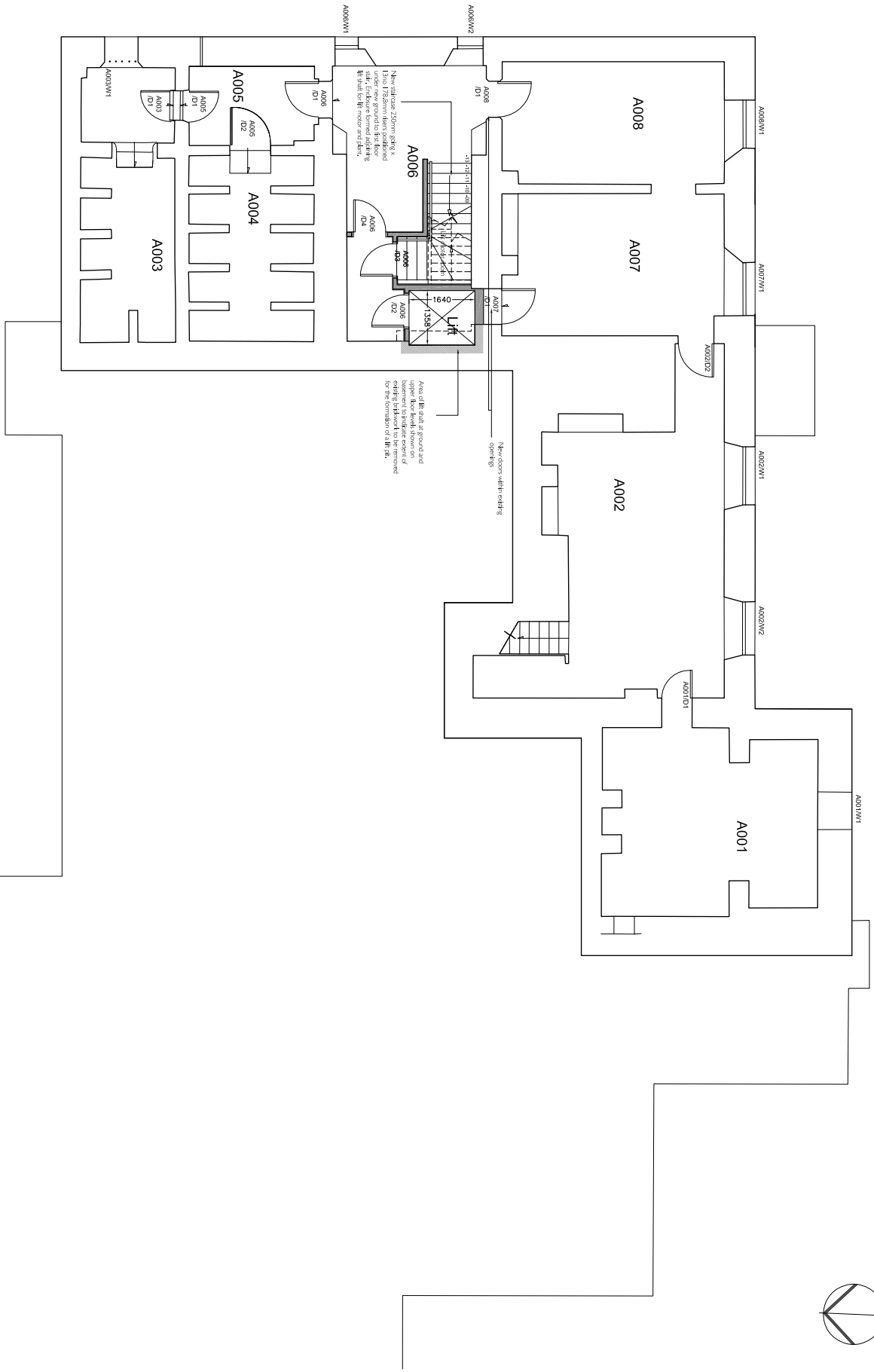
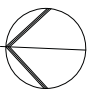
177 BRIDGDALE
STONINGWAY • LONDON • SE16 4QH
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PROJECT
ENFIELD
FORTY HALL
HLF STAGE 2

ISSUED	DATE	SCALE	CREATED
PROJECT	06.01.07	1:100 (A1)	14.00.07/AN
REV	04.11.07	Scale 1:1	1

GROUND FLOOR PLAN
PROPOSED OVERALL

DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND DESIGNER TO BE REFERRED TO THE ARCHITECT.



I BMRWard return at A006W.D1 16.08.09
 revised

H IBC application 14.08.09
 G Salf configuration retd to salf 11.07.09
 F Salf configuration retd to salf 11.07.09
 E in response to BIC comments.

STAGE D ISSUE
 E Stage D Issue 15.05.09
 D Fire door provision revised after 08.05.09
 C meeting with IQ and BIC 11.04.09
 B Door and window numbering 21.03.09
 A Additional detail and drawings for fire door and fire configuration 17.02.09

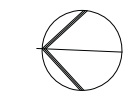
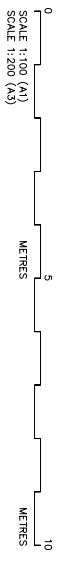
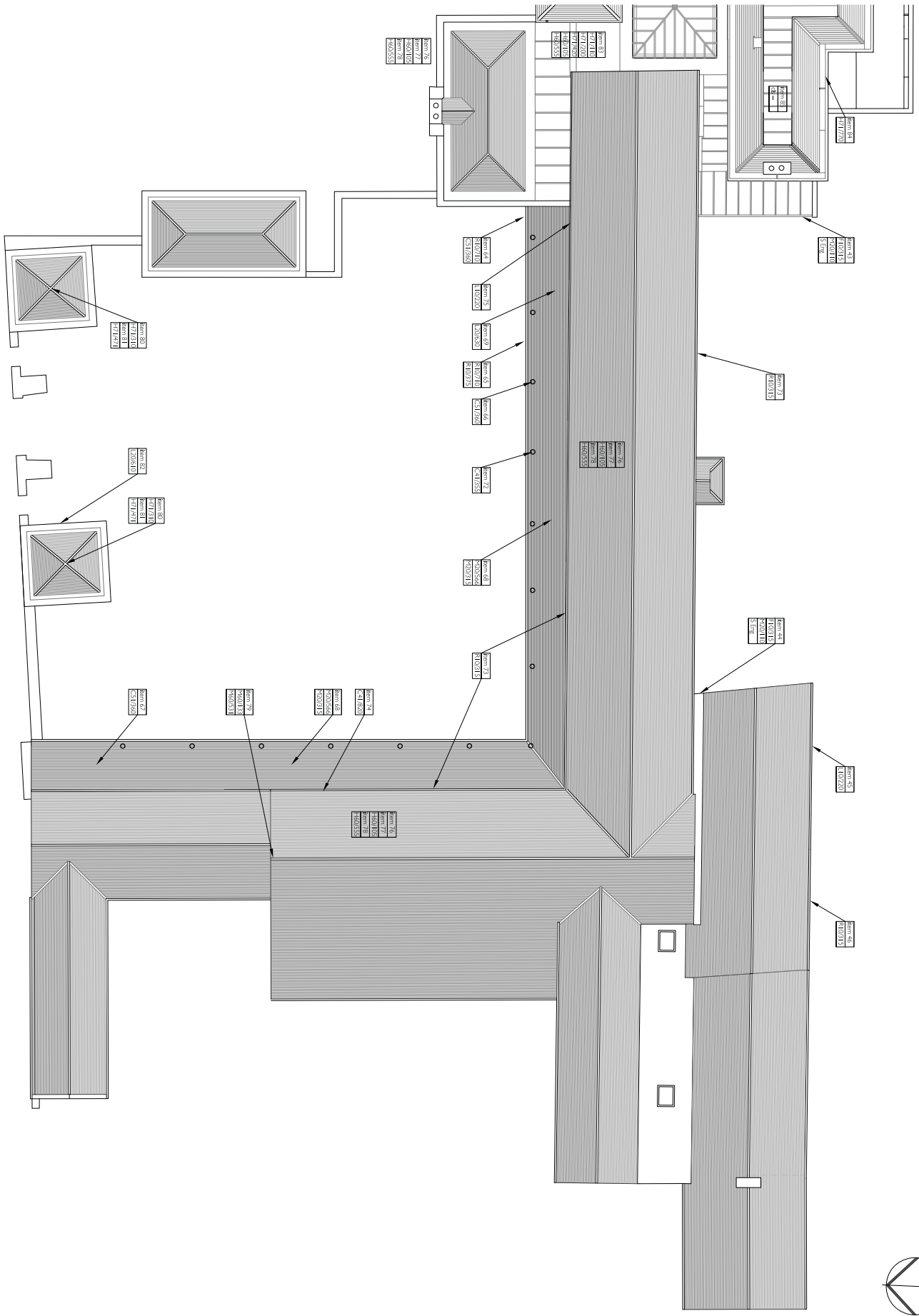
REV.	AMENDMENT	DATE

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 TEL: 020 7629 3230 • FAX: 020 7629 3146
 EMAIL: info@thomasford.com

PROJECT
 ENFIELD
 FORTY HALL
 HL1 STAGE 2

ISSUED	DATE	SCALE	CREATED
PROJECT	06.01.09	1:50 (A1)	
REV	04.10		

TITLE
 BASEMENT PLAN
 PROPOSED



DO NOT SCALE DIMENSIONS, THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE FOR THE ACCURACY OF ALL DIMENSIONS. DIMENSIONS TO BE REFERRED TO THE ARCHITECT.

Item	SCHEDULE OF WORKS/ITEM REF.
1	MS WORKS SPECIFICATION REF.
2	
3	
4	
5	
6	
7	
8	
9	
10	

LOCATING SYMBOL

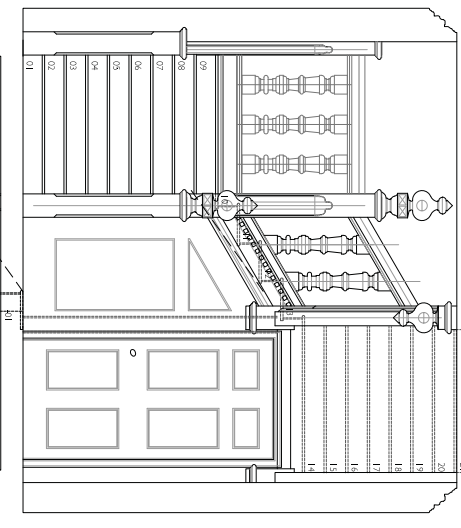
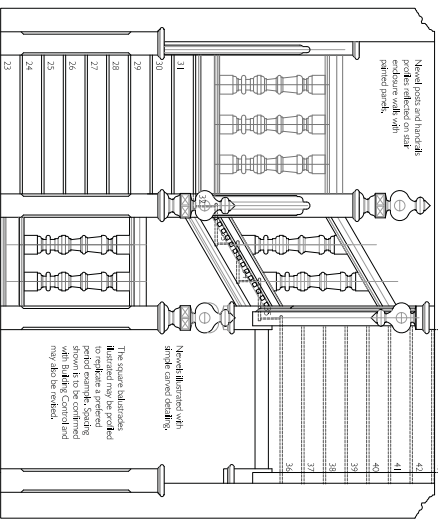
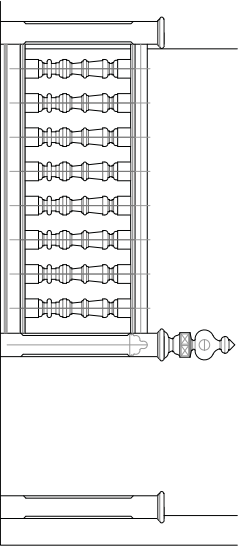
LBC APPLICATION		1:4/08/09
C LBC Application		
STAGE D ISSUE		
B	Stage D Issue	1:5/08/09
A	Items 43, 68, 69, 78, 79, and 08/08/09	
	08/03 stage ref: not refered.	
REV.	AMENDMENT	DATE

THOMAS FORD & PARTNERS
CHARTERED ARCHITECTS SURVEYORS

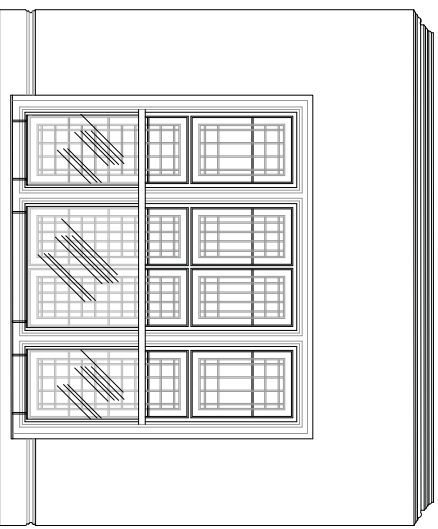
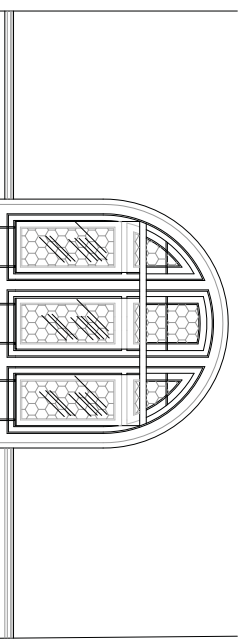
177 BRIDGEMERE
 STONEMAN • LONDON • SE16 4QH
 TEL: 020 7664 3230 • FAX: 020 7669 7146
 E-MAIL: info@thomasford.com

PROJECT
 ENFIELD FORTY HALL
 HLF STAGE 2

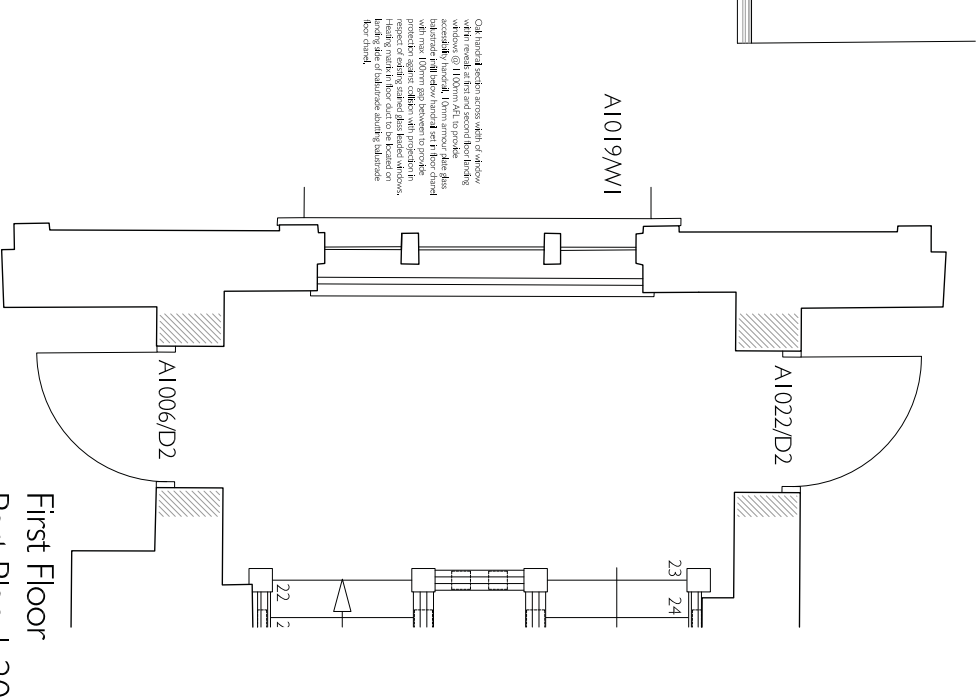
TITLE	ROOF PLAN PROPOSED ANCIILLARY BUILDINGS
PROJECT NO.	06.03.09
SCALE	1:100 (A1)
DATE	06/22
DESIGNER	T.F.
CHECKED	C.
DATE	06/22



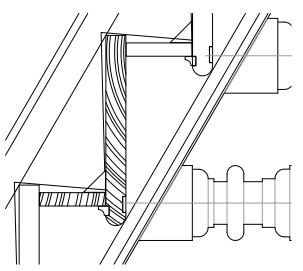
Section/View
A-A | 1:20



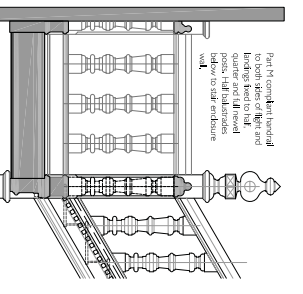
North Internal
Elevation | 1:20



First Floor
Part Plan | 1:20



Tread
Detail | 1:5



Landing
Section | 1:20

SCALE 1:50 (A1)
SCALE 1:100 (A2)
METRES 5 10

DO NOT SCALE DRAWINGS. THE CONTRACTOR AND DESIGNER IS TO BE REFERRED TO THE ARCHITECT.
SCHEDULE OF WORKS ITEM REF. NETWORKS SPECIFICATION REF. LOCATION NETWORK.

NO.	REV.	DATE
A	16.08.09	
Landing window furniture		
LBC APPLICATION		

THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS
177 BRIDGEMAN • LONDON • SE8 4QH
TEL: 020 7994 3230 • FAX: 020 7993 9146
EMAIL: info@thomasford.co.uk

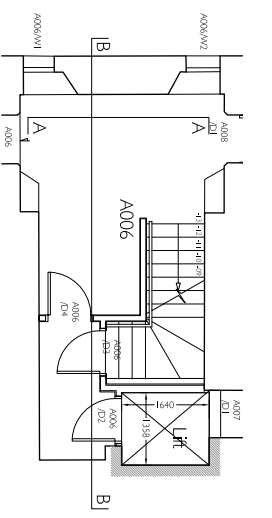
PROJECT	ENFIELD FORTY HALL HLF STAGE 2
TITLE	MAIN STAIR AS PROPOSED
DATE	2. of 2
SCALE	A
PROJECT NO.	25.04.09
DRAWING NO.	135030A1
DATE	04.05
DESIGNER	AS
CHECKED	A

Call handrail section across width of window
whilst reveal at top and second floor landing
balustrade with below handrail set in floor level
with new 100mm gap between to provide
protection against children with protection in
specification of handrail balustrade section,
handrail meets in floor set to be based on
top of call.

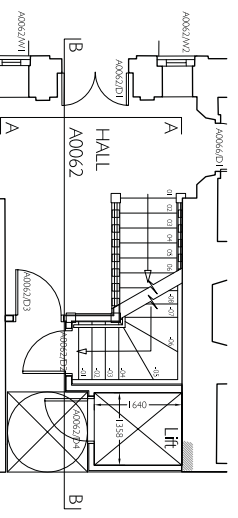
Call handrail section across width of window
whilst reveal at top and second floor landing
balustrade with below handrail set in floor level
with new 100mm gap between to provide
protection against children with protection in
specification of handrail balustrade section,
handrail meets in floor set to be based on
top of call.



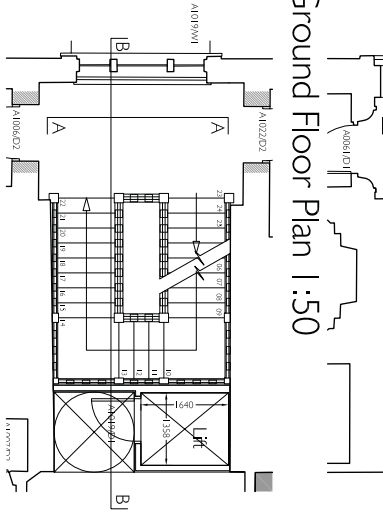
Section/View
B-B 1:20



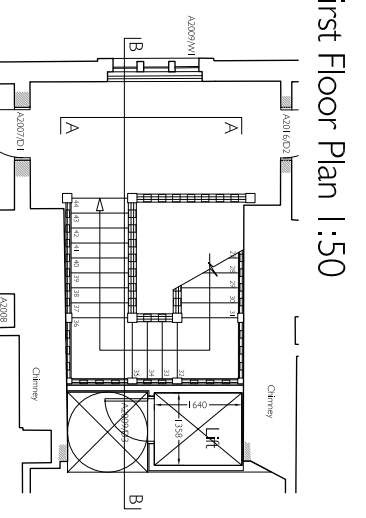
Basement Plan 1:50



Ground Floor Plan 1:50



First Floor Plan 1:50



Second Floor Plan 1:50

0 5 10 METRES
SCALE 1:50 (A1)
SCALE 1:100 (A3)

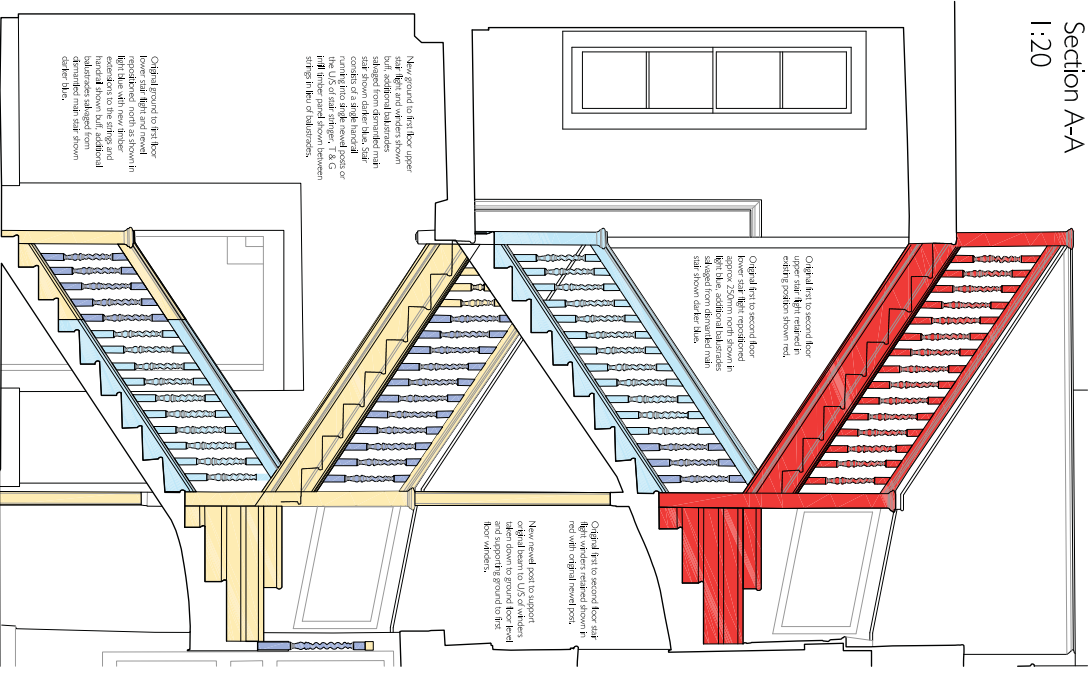
DO NOT SCALE DIMENSIONS, THE CONTRACTOR
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ARCHITECT.
LOCATION: 0909000

REV	DATE	SCHEDULE OF WORKS/CONSTRUCTION REF.	MS WORKS SPECIFICATION REF.
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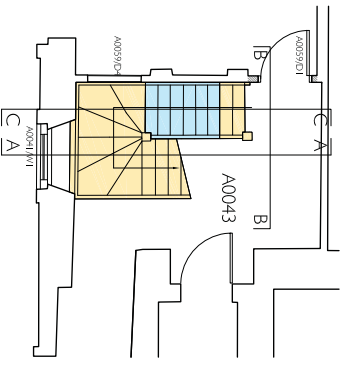
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A	15/05/09	Stage D Issue		
B	11/05/09	Stage D Issue		
C	11/05/09	Stage D Issue		
D	11/05/09	Stage D Issue		
E	11/05/09	Stage D Issue		
F	11/05/09	Stage D Issue		

THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS	
177 BRIDGE STONEMAN • CLONDON • SE16 4QH TEL: 020 7496 3200 • FAX: 020 7893 9146 EMAIL: info@thomasford.co.uk	PROJECT ENFIELD FORTY HALL HL1 STAGE 2
TITLE MAIN STAIR AS PROPOSED 1 OF 2	DESIGNED ANM 25/04/09 1:50/00/01 CHECKED GJM 25/04/09 1:50/00/01 DATE 25/04/09

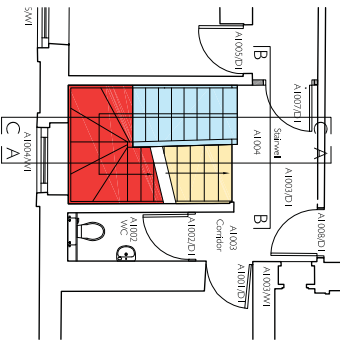
Section A-A
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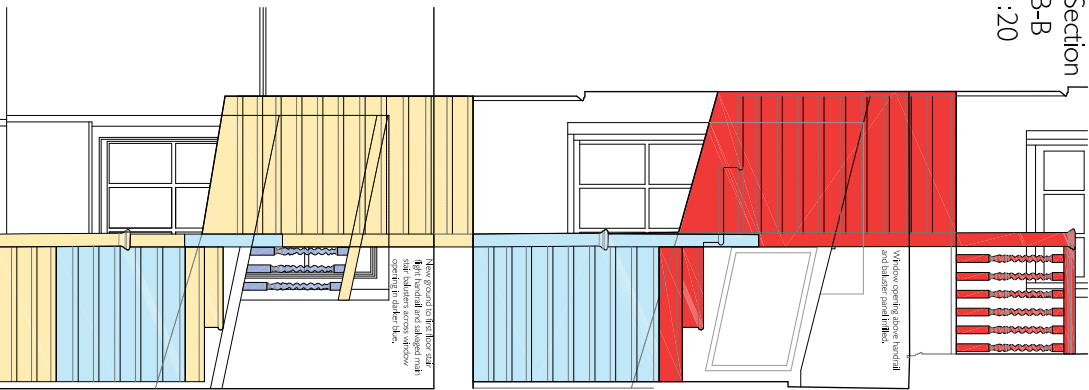
Ground Floor Plan 1:50



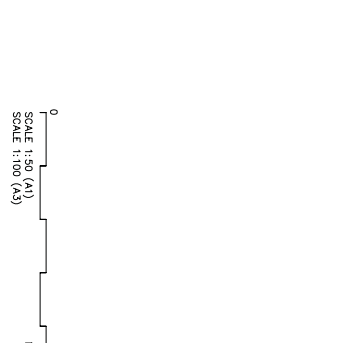
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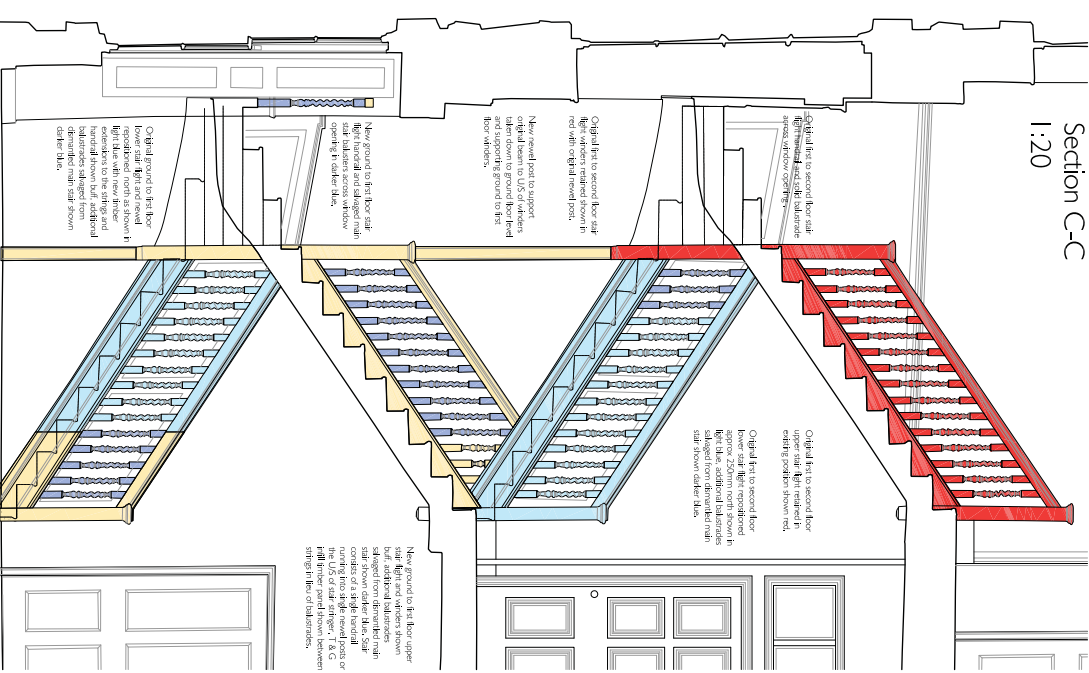
Section B-B
1:20



Second Floor Plan 1:50



Section C-C
1:20



DO NOT SCALE DRAWINGS. THE CONTRACTOR SHALL VERIFY DIMENSIONS AND CHECK FOR CONFORMANCE WITH THE SPECIFICATION. ANY DISCREPANCY IS TO BE REFERRED TO THE ARCHITECT.

SCHEDULE OF WORKS ITEM REF. NBS WORKS SPECIFICATION REF.

Item No	Description	Location	Area
1	Mezzanine opening	Mezzanine	1.2m x 1.2m

LOCATION: 18.06.2018

E Historical and modelling work: 18.06.2018

LBC APPLICATION

- D LBC application: 14.06.19
- C Drawing board and scales read: 06.06.19
- B Information, drawing board and plan: 31.07.19
- A Original files and revisions: 31.07.19

STAGE D ISSUE

A Stage D Issue 15.05.19

THOMAS FORD & PARTNERS
CHARTERED ARCHITECTS
SURVEYORS

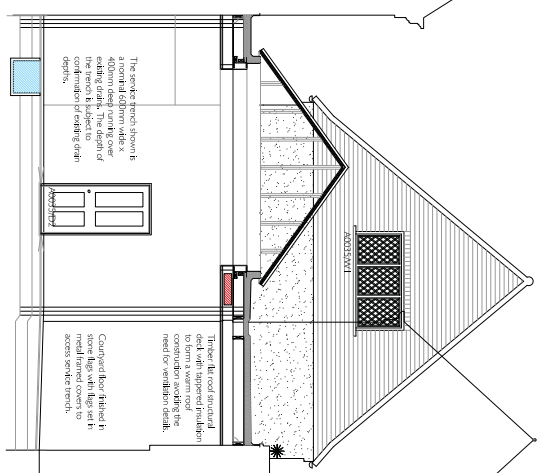
17 BIRKDALE
STOBNEW • LONDON • SE8 5QH
TEL: 020 7592 4720
WWW.THOMASFORDARCHITECTS.COM
EMAIL: info@thomasford.com

PROJECT		ENFIELD FORTY HALL HLF STAGE Z	
TITLE		REAR STAIR AS PROPOSED	
NO. 1	DATE	NO. 2	DATE
ANR1	21.04.19	1506BAN	18.06.19
BA1307	CA-22	C-22	E

DO NOT SCALE DRAWINGS. THE CONTRACTOR AND ARCHITECT SHALL BE RESPONSIBLE TO THE ARCHITECT.

SCHEDULE OF WORKS/CONSTRUCTION REF.	
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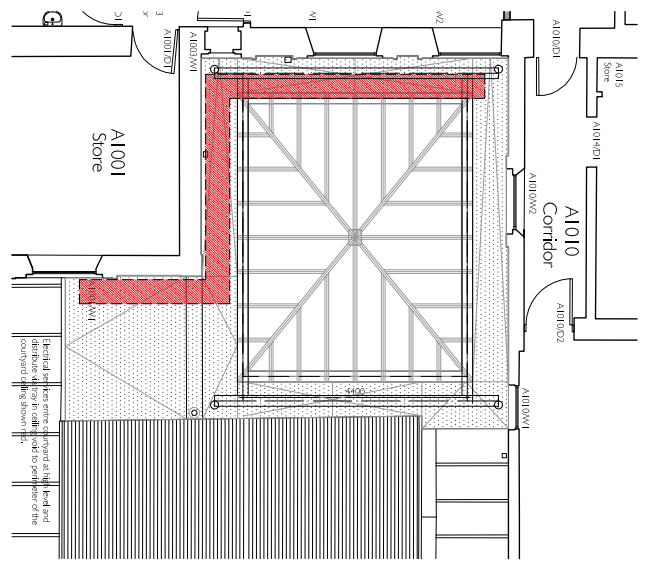
LOCATION: 298/200A



East Elevation/Section 1:50

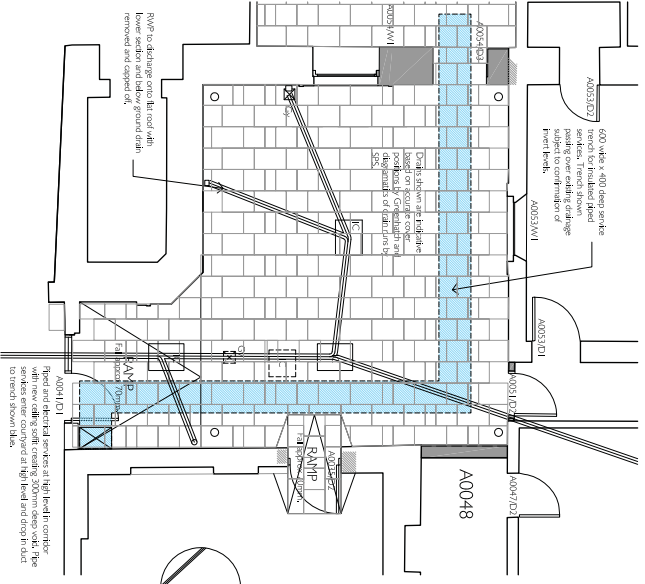
The steel structure above the window is shown in red. The steel structure is shown in red. The steel structure is shown in red.

Compare that the steel structure is shown in red. The steel structure is shown in red.



Part First Floor/Roof Plan 1:50

Detail of the steel structure shown in red. The steel structure is shown in red.

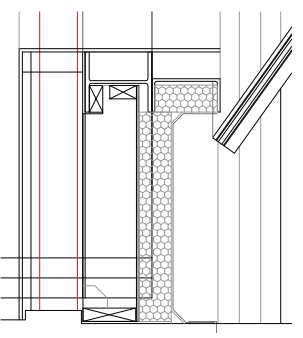


Part Ground Floor Plan 1:50

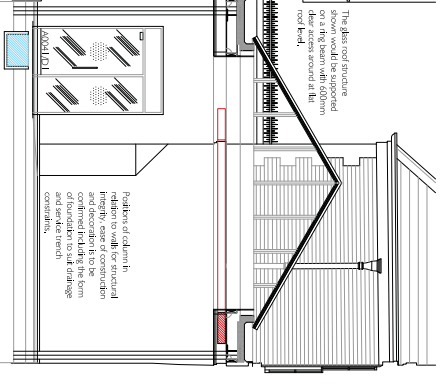
Ripe to be changed to be roof with... removed and replaced with...

Fixed and electrical services at high level in center with new ceiling... 300mm deep work. Ripe to be changed to be roof with...

The roof construction will be between perimeter of... 300mm deep work with 400mm... on 'V' type 'sawtooth' warm roof... of structural steel with secondary... detail. The head of timber joists on hangers forming supporting steel structure between... and... structure walls.



Detail Section 1:10

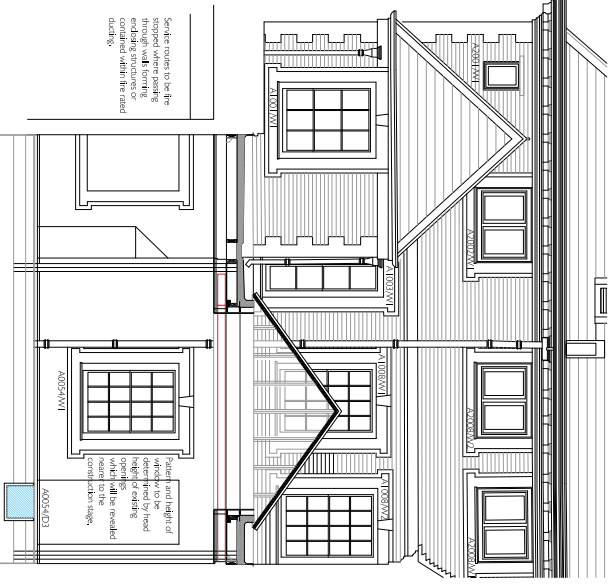


North Elevation/Section 1:50

Part of the steel structure shown in red. The steel structure is shown in red.

Part of the steel structure shown in red. The steel structure is shown in red.

Part of the steel structure shown in red. The steel structure is shown in red.



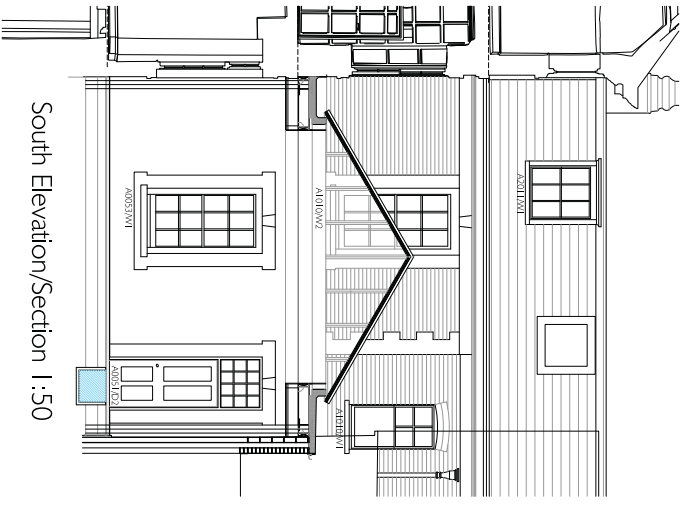
West Elevation/Section 1:50

Steel joists to be... through wall forming... ceiling structure... ducting.

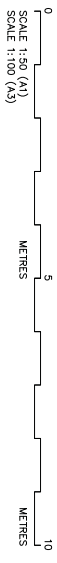
Part of the steel structure shown in red. The steel structure is shown in red.

Part of the steel structure shown in red. The steel structure is shown in red.

Part of the steel structure shown in red. The steel structure is shown in red.



South Elevation/Section 1:50



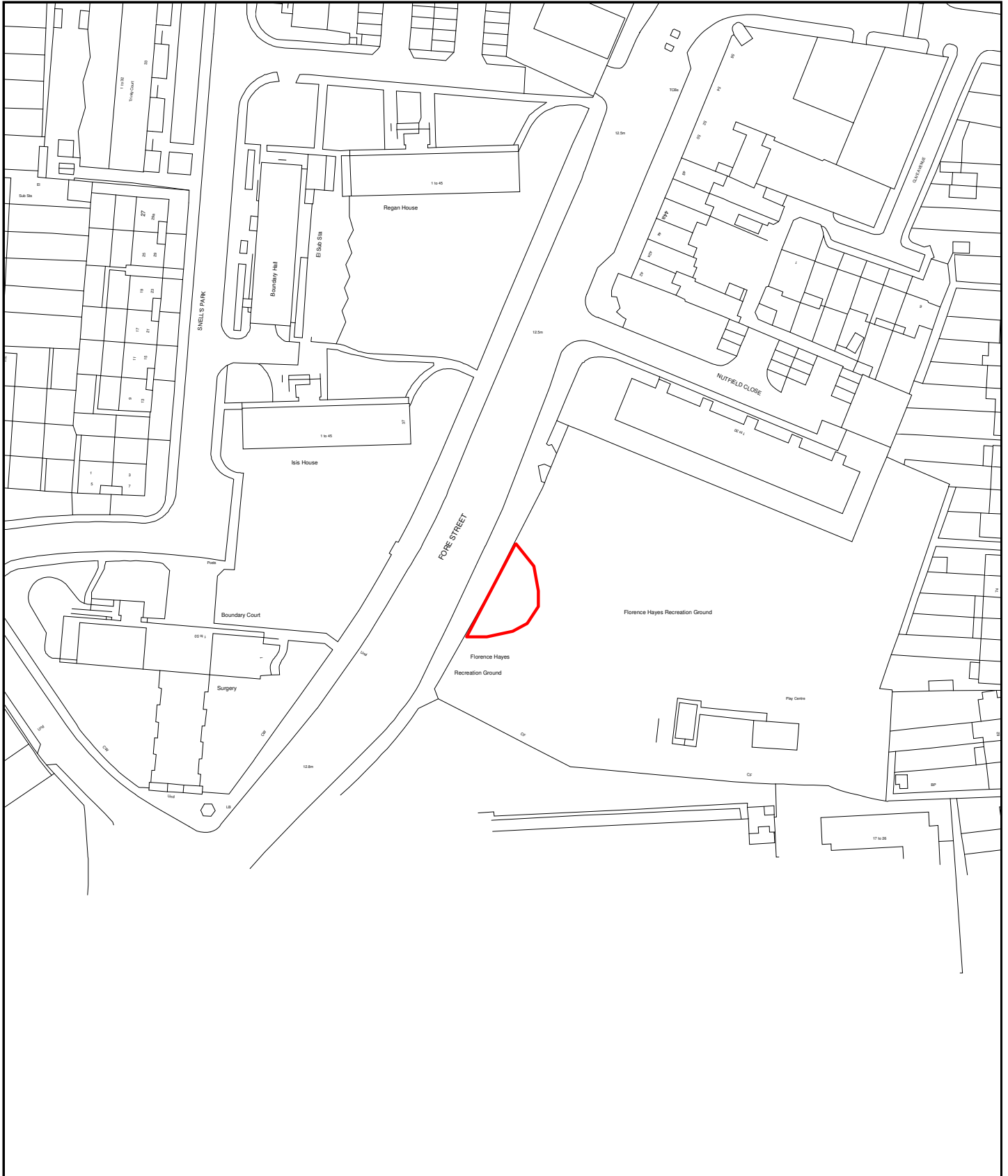
SCALE 1:50 (A1)
SCALE 1:50 (A3)

METRES

METRES

<p>THOMAS FORD & PARTNERS CHARTERED ARCHITECTS SURVEYORS</p> <p>177 BRIDGEMAN STONEMAN • LONDON • SE16 4QH TEL: 020 7661 3230 • FAX: 020 7897 3146 E-MAIL: info@thomasford.co.uk</p>		<p>PROJECT ENFIELD FORTY HALL HLF STAGE 2</p>	
<p>TITLE COURTYARD DETAILED AS PROPOSED</p>			
<p>DATE 06.08.09</p>	<p>SCALE 1:50 (SNA)</p>	<p>CREATED 06.26</p>	<p>REV 8</p>
<p>DRAWN 8/11/02</p>	<p>CHECKED 06/26</p>	<p>DATE 06/26</p>	<p>REV 8</p>

<p>NO. 8 Door A0048 (D1) and A0031 (D2) 16/06/09 revised.</p>	<p>DATE</p>
<p>A. LBC application</p>	<p>DATE</p>



Development Control



Scale - 1:1250
Time of plot: 15:28

Date of plot: 01/12/2009

Application Number: LBE/09/0036 **Ward:** Upper Edmonton
Date of Registration: 12th November 2009

Contact: David Warden 3931

Location: FLORENCE HAYES RECREATION GROUND, FORE STREET, LONDON, N18 2SP

Proposal: Installation of public art gateway feature with associated works.

Applicant Name & Address:

Mathew Havill
London Borough of Enfield
Highway Services, Carterhatch Depo
7, Melling Drive
Enfield
Middx
EN1 4BS

Agent Name & Address:

Recommendation: That planning permission be deemed to be **GRANTED** in accordance with Regulation 3 of the Town & Country Planning General Regulations 1992 subject to the following condition(s):

1. The materials used within the development shall reflect those detailed within the approved plans, unless otherwise agreed in writing with the Local Planning Authority.

Reason: To ensure a satisfactory external appearance.

2. C51A Time Limited Permission

Site and Surroundings

The application site is located on the east side of Fore Street, approximately 15 metres north of the Borough boundary with Haringey and a part of the Florence Hayes Recreation Ground where it adjoins the back edge of the footway.

The recreation ground is currently being redeveloped to provide a new play centre building, hard and soft play areas and an adventure playground. The surrounding area is predominately residential in character, with commercial and retail uses fronting Fore Street to the north and High Road to the south.

Proposal

Permission is sought for the erection of a piece of public art to provide a gateway feature to the Borough and the improved Florence Hayes Recreation Ground. A small area at the front of the Recreation Ground will be incorporated into the public highway to provide a semi-circular public space within which seating will be provided.

The artwork comprises a 3 metre high cylindrical stainless steel plinth, 'perched' on top of which will be a life size draught horse. The horse will be leaning over towards the base of the plinth, looking at a bronze cat that is reaching up towards the horse. The horse will be constructed of

thick stainless steel wire that will be shaped and welded to provide a hollow mesh structure that will include flowers and other shapes where the stainless steel wire overlaps. The total height of the structure will be 5 metres.

The artist's submission states "A sculpture that acts as a metaphor for working together and is a potent symbol for Enfield. As a gateway piece it will announce arrival in a creative, culturally rich and diverse Enfield. As a focal point sculpture it will be a source of local pride, engaging with people, celebrating friendship and a strong sense of place".

The selected artwork has been the result of a commissioning process involving an Arts Consultant, Council Officers, Ward Councillors, local interest groups and local schools. The brief for the piece required that the winning design must contribute to the social and physical identity of the area, animating the public realm and creating a sense of place to signify "your arrival in Enfield".

The scheme is promoted by Highway Services and the gateway feature will be sited alongside the existing public footpath within a newly created semi-circular paved seating area. The paved seating area will be enhanced by 2.7 metre high panels depicting the history of the site. These panels will be the culmination of a local history project with two local schools and shall describe the community facilities of the area over the last 300 years. Finally, extensive tree planting and new shrubs add structure and texture to the Gateway and create a visual impact both from an intimate perspective and from a distance.

Relevant Planning Decisions

LBE/08/0007 Redevelopment of public recreation ground to provide a new play centre building, hard and soft play areas, an adventure play ground area, new access gates to Fore Street and new chain link fence to boundary, granted in September 2008.

Consultation

Public

Consultation letters issued to 319 neighbouring properties. At the time of writing no replies have been received.

External

Any replies will be reported at the meeting

Internal

Any replies will be reported at the meeting

Relevant Policies

London Plan (2008)

- 3A.17 Addressing the needs of London's diverse population
- 3A.18 Protection and enhancement of social infrastructure and community facilities
- 3C.21 Improving conditions for walking
- 3C.22 Improving Conditions for Cycling
- 3D.4 Development and promotion of arts and culture
- 3D.8 Realising the value of open space and green infrastructure

- 4A.3 Sustainable Design and Construction
- 4B.2 Promoting world-class architecture and design
- 4B.3 Enhancing the quality of the public realm
- 4B.5 Creating an inclusive environment
- 4B.6 Safety, security and fire prevention and protection
- 4B.8 Respect local context and communities

Unitary Development Plan

- (I)GD1 Regard to Surroundings / Integrated into Local Community
- (I)GD2 Quality of Life and Visual Amenity
- (II)GD3 Character / Design
- (II)GD6 Traffic Generation
- (II)GD8 Site Access and Servicing

Local Development Framework - Core Strategy Preferred Options

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. The Core Strategy is at an early stage in its adoption process. As this continues the weight given to it will grow and the relevant objectives are reported to demonstrate the degree to which the proposals are consistent with the emerging policy direction.

- SO1 Sustainability and Climate Change
- SO2 Biodiversity
- SO3 Protect and enhance Enfield's environmental quality;
- SO11 Safer and stronger communities
- SO16 Preserve the local distinctiveness
- SO17 Safeguard established communities and the quality of the local environment
- SO21 Sustainable Transport

Other Material Considerations

- PPS1 Delivering Sustainable Communities
- PPS3 Housing
- PPG13 Transport

Analysis

Principle

The scheme would provide improvements to the public realm by providing a gateway feature to the Borough. The principle of development is, therefore, considered acceptable.

Character and Appearance

The proposal will be sited in a prominent position on Fore Street. Having regard to its presence in the street scene, it is considered that the proposal, at 5 metres in height, would not be inappropriate in terms of its scale. Furthermore, the open nature of the design will ensure that the proposal is not overly bulking or dominant. The structure also allows for flowered or other small designs to be added to horse where the thick wires that make up the structure cross to add interest.

The presence of a smaller piece, the cat, at ground level provides for greater interest to pedestrians and those seated within the new public space. The artist has confirmed that as the bronze ages, different colours will develop as parts of the cat, more than others, will be touched and become smooth.

The artist has confirmed that the inspiration for the choice of a draught horse relates to the individual Florence Hayes and her use of such horses to assist pulling carts up toward Edmonton. In addition, this element of the proposal provides links to the recent past where horses provided the primary means of transporting goods; something that presently remains within living memory of many. The use of a cat relates to Florence Hayes' good work with animals.

The very nature of selecting a piece of public art is such that opinions are clearly subjective ones and opinion is often mixed. However, there was a clear consensus within the commissioning process that this piece provided the best mix of positive symbology, appropriate materials and scale and the potential to engage with a range of people and age groups.

Overall, it is considered that the project and the centre piece will provide a quality public space and gateway feature that will have a positive impact on the character and appearance of the area.

Impact on Neighbouring Properties

Potentially, the seating and public space may act as a focal point for people to congregate. However, the application site is located approximately 50 metres from the nearest residential property and adjacent to both the busy Fore Street and the improved public open space. As such, having particular regard to the existing level of activity within the immediate vicinity of the site, it is not considered the proposal would adversely affect the amenities of nearby residents or the wider area.

Access and Construction

There are no highway safety implications arising from the proposal other than those associated during the construction and future maintenance of the artwork. However, these would be of a temporary and infrequent nature and it is considered, they would not give rise to conditions notwithstanding the fact that Fore Street is a heavily trafficked strategic transport route with a bus lane and stop in close proximity that would adversely affect the free flow of traffic or highway safety.

Other Matters

It is relevant to consider the extent to which the proposal will stand up to the pressures of being within the public domain, in particular, vandalism, and graffiti and, in respect of the bronze cat, theft.

The chosen materials are robust and are commonly used for pieces of public art. In addition, the materials are easily pressure washed to remove graffiti and, the design of the artwork provides for limited flat surfaces. Assurances have been received regarding the stability of the piece and the height of the structure and nature of its design does not easily lend itself to being climbed. Furthermore, the scale of the main stainless steel structure and the requirement for a crane to move it limit the potential for theft. However, in respect of the bronze cat, there are some concerns regarding the potential for theft. The artist has provided assurance that this element of the scheme will be secured at a number of points where it is in contact with the ground or the cylindrical plinth. In addition, the risk of theft must also be balanced against the desire to use high quality and aesthetically pleasing materials within the public domain. Consequently, it is considered, having regard to the sites location on a busy pedestrian and vehicular route and the

fixing of the bronze discussed above, the risks are not outweighed by the benefits derived from the scheme.

Conclusion

In the light of the above assessment, it is considered that the proposal will provide for an aesthetically pleasing and attention grabbing gateway feature that will provide a future landmark and signify arrival within the Borough. According it is recommended that planning permission is granted for the following reasons:

- 1 The proposed installation of public art gateway feature with associated works would not detract from the character and appearance or the visual amenities of the surrounding area, having regard to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan, as well as London Plan (2008) Policies 3D.4, 3D.8, 4B.2, 4B.3, 4B.5, 4B.6 and 4B.8 and the objectives of PPS1, PPS3 and PPG15.
- 2 The proposed installation of public art gateway feature with associated works would not unduly affect the amenities of adjoining or nearby residential properties having regard to Policies (I)GD1 and (I)GD2 of the Unitary Development Plan, as well as the objectives of PPS1
- 3 The proposed installation of public art gateway feature with associated works would not give rise to unacceptable congestion or highway safety issues, having regard to Policies (II)GD6 and (II)GD8, as well as Policy 3C.21 of the London Plan and the objectives of PPG13.

NOTES

- DO NOT SCALE FROM THIS DRAWING.
- ANY ERRORS OR OMISSIONS TO BE IMMEDIATELY FOUND.
- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.
- ALL LEVELS ARE IN METRES AND RELATE TO ORDNANCE DATUM LEVEL UNLESS NOTED OTHERWISE.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS IN THE SAME SERIES.

KEY

- NEW DOUBLE LEAF ACCESS GATE
- NEW PERISTYLE GATE
- PHASE 1 - 1.8m HIGH POST AND RAIL FENCE
- PHASE 2 - 1.8m HIGH POST AND RAIL FENCE MARKED ON EXISTING RAISED PLANTER
- PHASE 3 - 1.8m HIGH POST AND RAIL FENCE
- EXISTING ASP FOOTWAY
- EXISTING TARMAC FOOTWAY
- PLANTER TREES INSTALLED BY OTHERS PRIOR TO FENCING WORKS
- PROPOSED GROUND LEVEL BENEATH FENCE

NOTE:
RESPONSIBLE OFFICER TO VERIFY THE LOCATION AND REDUCED LEVELS OF DATUM POINTS
FINISHED LEVELS MAY BE SUBJECT TO CHANGE

No.	Revision details	Date

QUOTATION

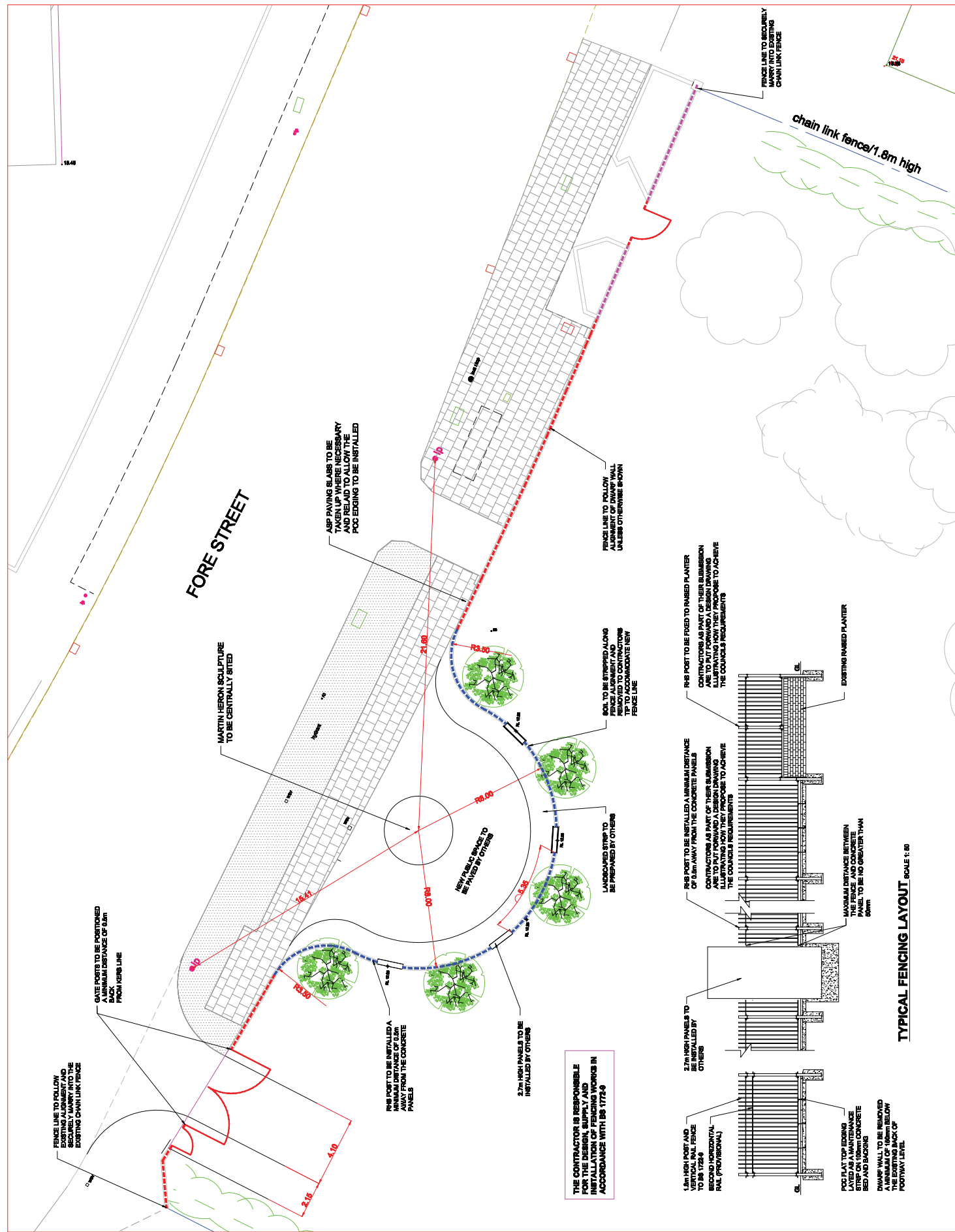
CLIENT
LONDON BOROUGH OF ENFIELD

JOB TITLE
FLORENCE HAYES RECREATION GROUND GATEWAY SCHEME

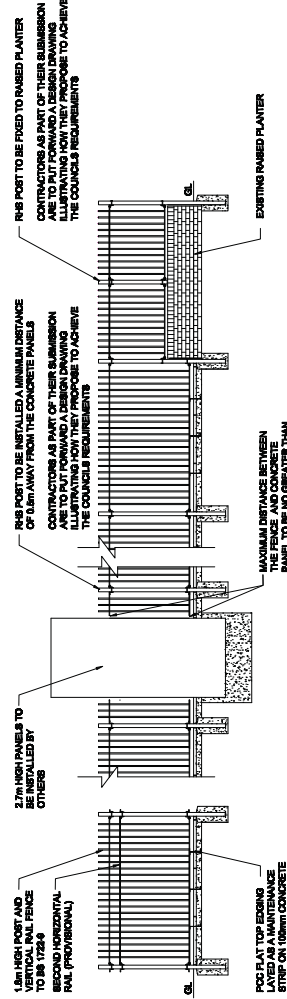
DRAWING TITLE
PROPOSED FENCING ALIGNMENT SHEET 2 OF 2

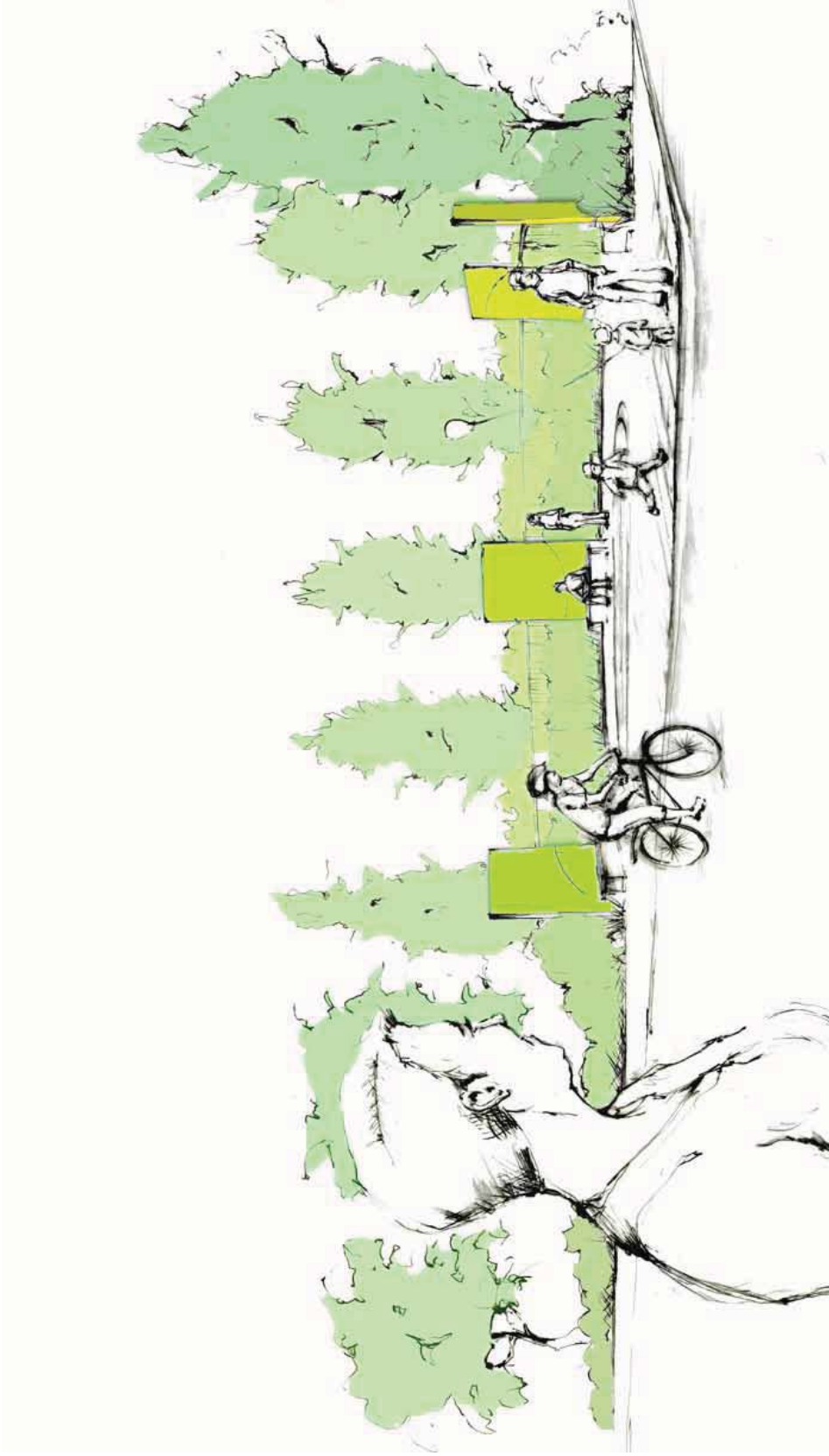


DATE: OCT 2009	SCALE: 1:1
DRAWING NO. 2009/127/02	SHEET 2 OF 2



THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN, SUPPLY AND INSTALLATION OF FENCING WORKS IN ACCORDANCE WITH BS 1173-9





ARTIST INFORMATION PACK: Public Art Commission for the Florence Hayes Gateway

Florence Hayes Recreation Ground is a small parcel of green space amid a harsh, bleak landscape of high-density social housing within the deprived North London Suburb of Upper Edmonton. This pocket of pasture aligns itself with Fore Street, a busy A road, which experiences a sea of pedestrian use every time Tottenham Hotspur Football Club play at their home ground less than a mile away. Most importantly, the boundary between London Borough of Haringey and London Borough of Enfield can be identified by this green gateway.

Originally purchased by the Council under the conditions of section 164 of the Public Health Act 1875, the recreation ground has remained in a poorly maintained condition for the last 50 years. It is now the subject of intense redevelopment in the aim to provide an area that is truly valued by the local community - in a way it was in previous generations. Phased development over the next year will see the creation of an adventure play area, a play centre with disabled facilities, a sensory garden and a community planting scheme.

In order to welcome people into Enfield from the South, the Redevelopment and Environmental Works team at Enfield's Highway Services have secured funding for an environmental improvement scheme that will be initiated to create a statement Gateway at Florence Hayes. The landscaped concept sketches developed by Redevelopment and Environmental Works, depict enhancements that include landscaped footway works and the creation of a semi circular seating area so pedestrians are able to take the time to enjoy their improved surroundings. Central to this design will be the creation of a piece of public art, which will sit on a central dais clearly visible from the Highway. Enfield Council invite you to register your interest in designing this focal point, which will be permanently displayed at the site and signify your arrival in Enfield.



Location plan, not to scale. Extents of Florence Hayes recreation ground shown in red, extents of proposed gateway shown in blue.

INTRODUCTION

Enfield Council has allocated capital funding for a number of environmental improvement schemes that aim to enhance the gateways into the London Borough of Enfield. The Redevelopment and Environmental Works team have developed a concept design of the scheme as a whole, including a new footway and semicircular seating area. An innovative piece of public art is sought to complement this vision and be the focal point of the Florence Hayes Gateway.

This commission should support Enfield Council's aim of "Putting Enfield First" in the following ways:

- Promote a cleaner greener Enfield
- Upgrade and improve Enfield's roads and pavements
- Integrated, flexible, creative and innovative Highway Services.

And should support Highway Services' Improvement Plan by:

- Improving the soft assets of the highway network (grass verges, shrub beds and trees).
- Enhancing the gateways of the borough.
- Creating a cleaner, greener and sustainable Enfield.

OVERVIEW

Florence Hayes recreation ground is on the boundary of Enfield and Haringey and is situated on Fore Street, Upper Edmonton, which is a vibrant and busy thoroughfare. A large estate of houses dominates the west side of Fore Street and flats, shops and residential housing flank the east side. The street is particularly active during match days at Tottenham Hotspur Football Club.

The winning design must contribute to the social and physical identity of the area, animating the public realm and creating a sense of place to signify "your arrival in Enfield."

In order to achieve this, Redevelopment and Environmental Works (Enfield Highways) are seeking to commission an artist to:

- Create a work that is high quality in concept and execution.
- That will enhance, promote and personalise the public space.
- Create recognition and distinction for the locality.
- Link the new developments with the local community

In addition the design should:

- Be robust and have longevity.
- Complement and integrate into the finished design scheme.
- Be low maintenance.
- Harmonise with the site- specific palette of materials.
- Be suitable for placing on the centre dais depicted in the concept plan provided.



The site as seen from the south



The existing bus stop

SITE PLAN AND INFORMATION

FLORENCE HAYES PLAY FACILITY

Works Programme

Phase 1 - Play Centre Building to be completed November 2009

Phase 2 - Adventure Play Ground to be completed March 2010

Project Description

Parks in partnership with Education, Highways and external agencies are building a new innovative play facility for children and young people (2-16years) at the Florence Hayes Recreation Ground, which is on the border of Enfield and Haringey.

Through a combination of Big Lottery and Play Pathfinder funding this project aims to bring life back into this largely disused space that was once used for play schemes located within an area that has one of the highest levels of deprivation within the borough.

The completed project will include a new Play Centre Building and Adventure Play Environment. The site will be divided into a series of activity areas which include an adventure play area, hard play area, community garden planting area, sensory garden and a woodland free play area.

We are actively encouraging the local children and young people to get involved with the design and building of the play spaces.



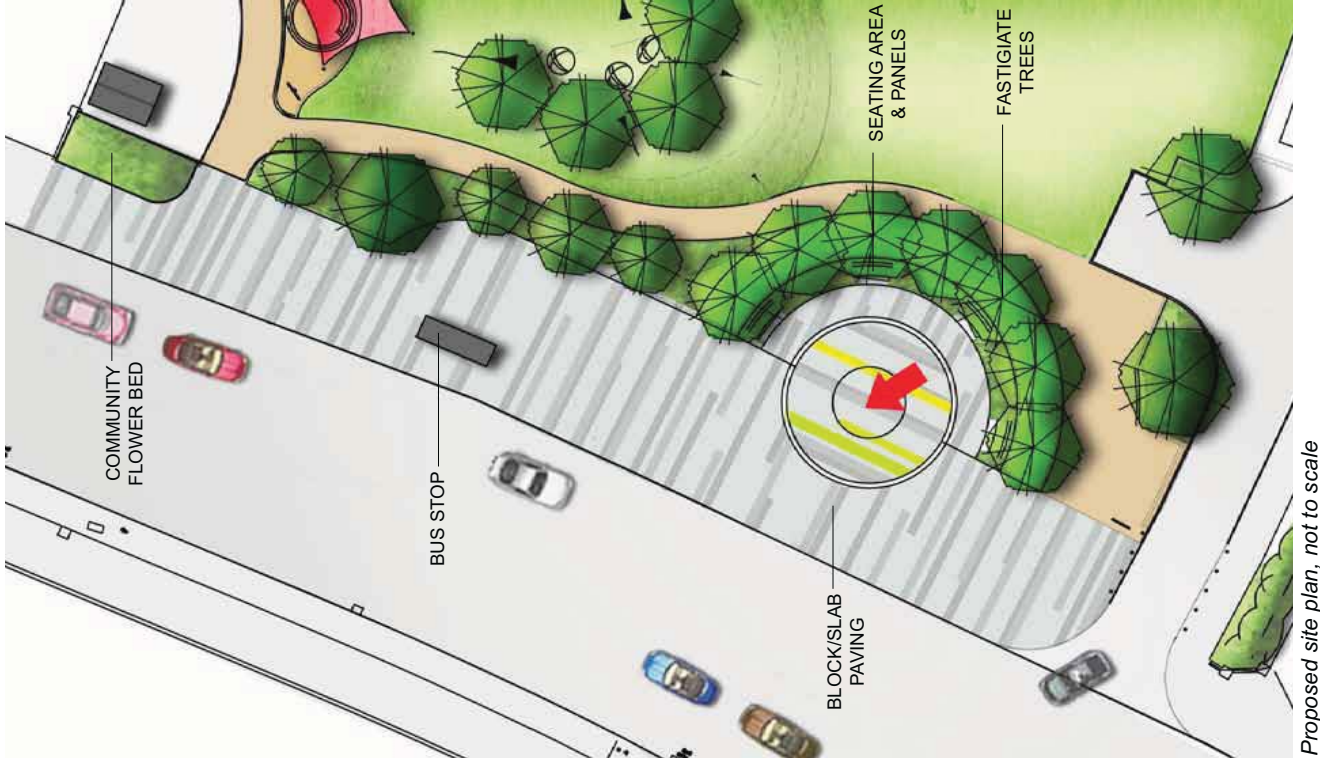
Proposed play facility at Florence Hayes Recreation Ground



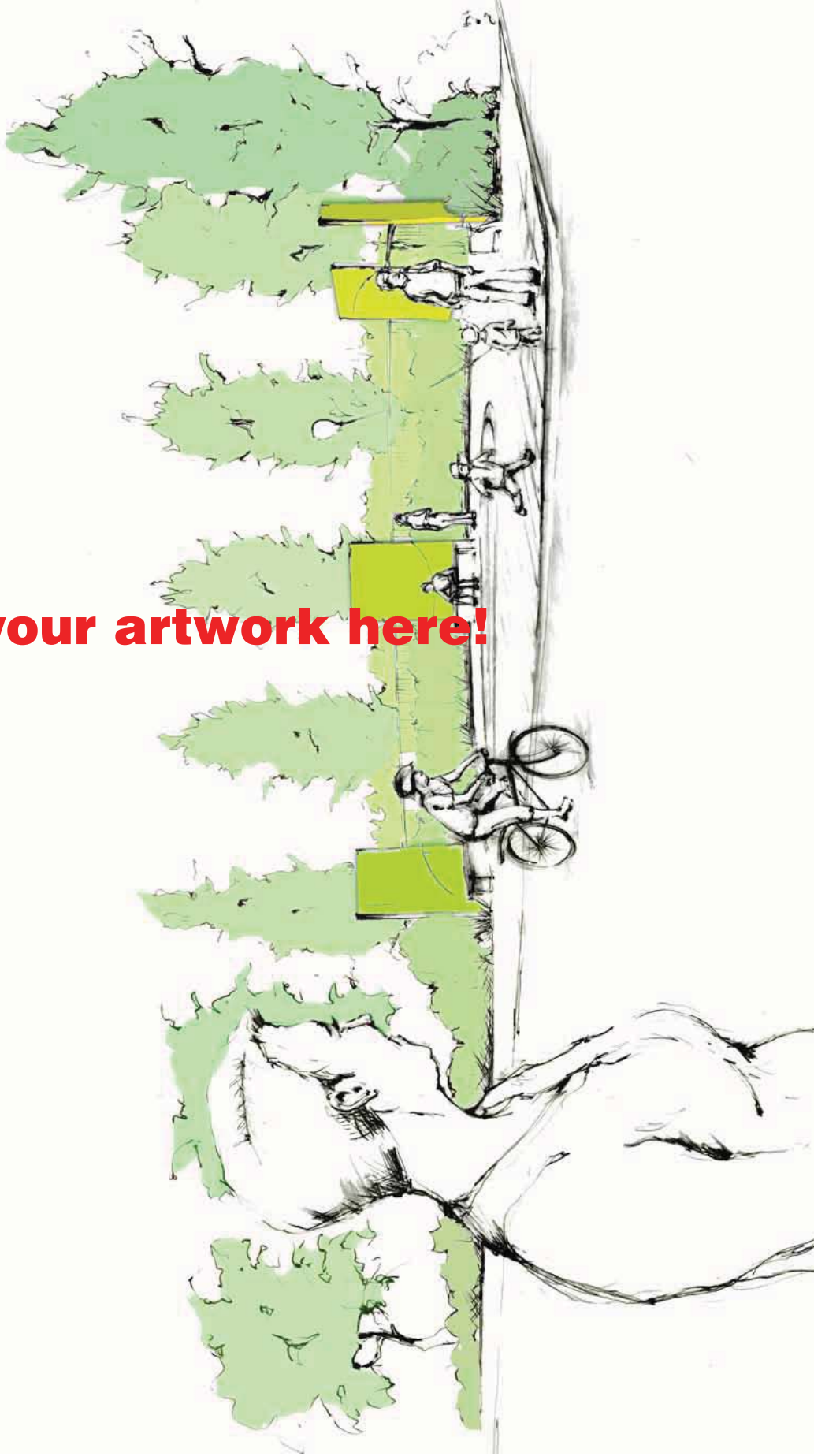
The circular space incorporated with the footway improvements on the western side, is the space reserved for the winning commission.

THE PROPOSED GATEWAY SCHEME

The successful design will act as the focal point of a scheme that has been conceptually developed to provide an enjoyable and educational space. High quality paving and seating will be enhanced by panels depicting the history of the site. Such panels will be the culmination of a local history project with two local schools and shall describe the community facilities of the area over the last 300 years. Finally, extensive tree planting and new shrubs add structure and texture to the Gateway and create a visual impact both from an intimate perspective and from a distance.



your artwork here!



HISTORICAL DESCRIPTION OF THE SITE

Florence Hayes

Florence Hayes was born in 1879 in Portsmouth to Alfred Hayes, a Sergeant in the Royal Marine Artillery, and Emma nee Sparkes. At the time of the 1881 census the family were living in the Royal Marine barracks at Eastney in Portsmouth. By 1891 the family were living in Southsea, Portsmouth, where Alfred worked as a coal merchant, later becoming a green grocer. They moved to Tottenham in 1892, when Florence was 13 years old.

Florence Hayes attended St Mary's Church Sunday School in Lansdowne Road, where she also eventually became a teacher, as well as the Superintendent for the Band of Hope (a temperance organisation founded in Leeds in 1847 for working class children, whereby the members had to sign a pledge to abstain from all alcoholic drinks). Her sister, Ada, was also a teacher in the area all her life, first teaching at St Paul's & All Hallows C.E. school in Tottenham; then, in 1920, eventually becoming headmistress at Raynham Road Girls School, Edmonton. Both sisters would travel back to Southsea every year together for their holiday.

Florence Hayes moved from Tottenham to Edmonton in 1905. By then she was well known for her good works' throughout the area. She was a great lover of animals and dedicated much of her life to helping them. She was a secretary of 'Our Dumb Friends League', later to become known as The Blue Cross, at Gilpin Grove. She broke her leg around 1948 and was an invalid at home after that until her death at the age of 76 on 19th June 1956.

The London Borough of Enfield and its predecessors purchased the land that today makes up Florence Hayes Recreation Ground in nine parts between 1957 and 1972. Some of these parts were bought by compulsory purchase order, pursuant to section 164 of the Public Health Act 1875, to provide a place of public recreation.

The land previously had many uses, including private dwellings, shops, a tobacconist and a public house (The Rifleman). The site also formed part of the grounds of the church, vestry and schoolrooms of The Tottenham and Edmonton Chapel, also known as Congregation Church

Detailed local history information will be made available to the shortlisted artists.

LOCAL AREA DEMOGRAPHICS

Upper Edmonton Ward:

Population: 14,843

11% over 65 years

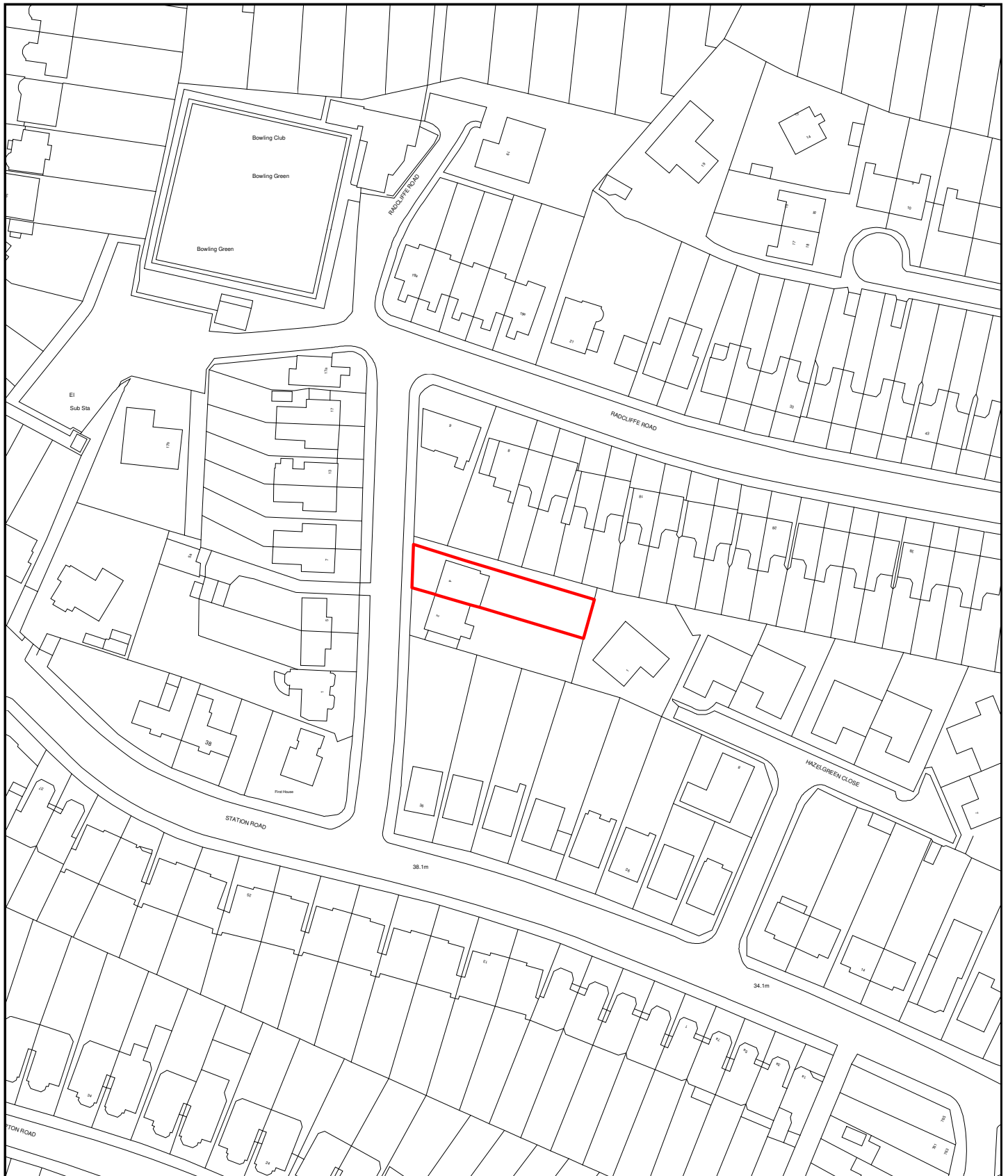
25% Under 15 years

Above average unemployment (590 unemployed compared to 389 as average per ward)

57% of community is from an ethnic minority (average is 37%)



Congregation Church, Fore Street, February 1965



Development Control



Scale - 1:1250
Time of plot: 16:26

Date of plot: 01/12/2009

Application Number: TP/09/0669 **Ward:** Winchmore Hill
Date of Registration: 28th May 2009

Contact: Emma Allenden 3845

Location: 4, RADCLIFFE ROAD, LONDON, N21 2SE

Proposal: Two storey side extension with integral garage, rear conservatory and basement parking.

Applicant Name & Address:

Mr S. Sotoriou
4, RADCLIFFE ROAD
LONDON
N21 2SE

Agent Name & Address:

Mr Michael Wallis, PMSS
5, HOUNSDEN ROAD
LONDON
N21 1LU

Note for Members

At the meeting of Planning Committee on 22nd September, it was agreed to defer determination of this application to enable officers to investigate any effects arising from the operation of the car lift in terms of noise and vibration and effects on the adjoining half of the semi detached property.

No additional information specifically pertaining to these issues has been produced although an offer has been extended to visit other examples of such lifts in operation.

Council Officers have also taken the opportunity to review the assessment of the proposal in terms of noise and vibration with colleagues in Environmental Health and Building Control. Environmental Health have confirmed that the specification provided show that the noise would be acceptable, with Building Control highlighting a car lift that was installed successfully in a property in Cockfosters Road in 2007 with no issues of vibration or other issues raised since then.

Accordingly, this application is reported back to Committee with a recommendation to approve.

Recommendation: That planning permission be **GRANTED** subject to the following conditions:

1. C08 Materials to Match
2. C09 Details of Hard Surfacing
3. C10 Details of Levels
4. C11 Details of Enclosure
5. C16 Private Vehicles Only - Parking Areas
6. C25 No additional Fenestration

7. C26 Restriction of Use of Extension Roofs
8. Car Lift
9. C51A Time Limited Permission

Site and Surroundings

The property is a semi detached single dwelling house on the eastern side of Radcliffe Road. The area is predominantly residential and is characterised by terraced, semi-detached and detached properties.

Proposal

Permission is sought for the construction of a two storey side extension to the property, 3.2 metres in width and abutting the boundary with the rear gardens of nos. 8 and 10 Radcliffe Road. This extension would provide a garage at ground floor level and additional bedroom space at first floor level. The proposal would also provide basement parking underneath the garage, accessed by an internal car lift.

A rear conservatory is proposed behind the garage projecting 2.1m beyond the existing rear wall of the property.

Relevant History

None.

Consultations

Public

Consultation letters have been sent to 11 neighbouring properties. Objections have been received from the occupiers of nos. 2, 5a, 6, 8, 10 and 16 Radcliffe Road raising all or some of the following points:

- The amended application has not overcome the concerns raised when the initial plans were submitted
- Noise and vibrations from the car lift – lack of justification that this lift will not have an adverse impact on neighbours, the examples submitted of other lifts are different locations and should not be used but a proper assessment of potential noise impact should be carried out
- Examples have been submitted of other applications in other locations where car lifts have been refused or withdrawn showing that there are potential impacts on adjoining and neighbouring uses from the noise and vibration likely to be emitted by a proposed internal activity in a development
- The car lift is a feature out of keeping with the locality and is not needed as adequate parking is available on the site without the need for the car lift
- These types of car lift system are not generally suited to frequent or short-term use, their primary purpose being to increase storage capacity in space-constrained sites
- The system incorporates low noise units. Nevertheless the manufacturer recommends that where parking systems are installed in a garage, the garage should be built separately from the dwelling

- A minimum of 62dB of sound insulation should be incorporated in the design in order to protect dwellings from unacceptable levels of noise – the above 3 points show that the scheme is not appropriate for a semi-detached property in a quiet residential street
- The height of the conservatory has been reduced but the roof height over the side extension has increased
- The proposal does not comply with Council policies (I)GD1, (I)GD2, (II)GD3, (II)H12, (I)EN6, (II)EN30
- It does not improve the quality of life and visual amenity and will result in a deterioration
- The scale and mass would be highly visually intrusive
- Detrimentially affect the outlook and amenity of adjoining residential properties, enjoyment of garden
- Two storey development is inappropriate having regard to the proximity of the backs of adjacent Radcliffe Road properties – this was acknowledged in the single storey bungalow development in Hazelgreen Close built in the mid-1980's – the existing flank wall of the application property is already closer to no. 8 Radcliffe Road than was allowed at the bungalow development – a letter submitted by the applicants for this development stated that the development requires careful consideration and if it is single storey will not be detrimental to the residents of Radcliffe Road and Station Road
- The development would not integrate into or improve the local community, and will prejudice the amenities of adjoining properties
- Encroachment over adjoining properties
- Inadequate distance has been left to allow for the maintenance of the dwelling from within the curtilage of the property
- Impact on trees and habitat
- The applicants dog keeps the occupier of no 5a Radcliffe Road awake at night already
- Loss of privacy
- Loss of light and impact on enjoyment of surrounding gardens from the extension
- Could set a precedent for the area
- Noisy building works from no. 11 Radcliffe Road are going on, this property is also owned by the applicant – do these works have planning permission?
- No site plan showing the adjoining site property boundaries has been submitted
- Two Land Registry titles should be submitted showing the application property and the alleyway running along the backs of 6 – 16 Radcliffe Road
- Could some of the work be constructed as permitted development?
- The forms submitted with the application are not accurate – questions 5 and 7 should be 'yes' as a vehicular access to the property would be needed and there would be an impact on trees
- Conditions have been suggested if the application is approved
- An example was submitted from no. 2 Radcliffe Road in relation to a planning application at Southwark Council that was refused for the following reason: In the absence of an acoustic report, it is considered that the proposed mechanical car docking system would generate excessive noise causing harm to the amenity of future and neighbouring occupiers. The proposal is therefore contrary to Policy 3.2 'Protection of Amenity' of the Southwark Plan UDP (2007). However, a revised application included an acoustic report which demonstrated that the Cardok system would not have a significant impact on noise, and in fact the noise produced by the system is far less than that of a car door slamming. This objection was therefore removed from the scheme and planning permission was granted for the car docking system.

The Winchmore Hill Residents Association have concerns that the side extension will unbalance the whole of the building; the car lift inhibits level access from front to rear garden; the lower car lift bay is only 1500 high making it difficult for personal access; elevational details of existing houses must be carried over on to any new extension; the garage door is a 'foreign' opening in

respect of its size and shape but it must follow the existing elevational details; the rear windows of the occupants of no. 6 Radcliffe Road will face onto the extension which will cause substantial over-shadowing to their garden; access for maintenance would have to be agreed with the adjoining owner; the hipped roof does not match the existing; which doors are to be used on the conservatory is not clear.

In addition, representations have been received from Cllr Prescott who objects to the application due to it being massively obtrusive to neighbouring properties; that parts of it extend over at least one other property; that the subterranean car park is wholly out of keeping with the locality; that the works will create enormous disruption and nuisance and may damage trees in the immediate vicinity; and that after the works have been completed the machinery and extractor fans will be a permanent reminder of the folly of this development.

Furthermore, a letter of objection has also been received from David Burrowes MP who expresses concerns that the proposed development would seem to be inappropriate for a semi-detached property in a quiet residential street; that the equipment is most often found in commercial properties or detached properties where the garage is separate to the house, and by granting this we will create a precedent for semi-detached properties; neighbours concerns should be taken into consideration, particularly in relation to the potential noise pollution and the suitability of the proposed equipment in residential properties.

Internal:

Environmental Health state that the having looked at the updated information on the website if the car lift can be designed to meet the noise levels stated then there should not be a problem with noise in the neighbour's property. The system should have an acoustic performance to the noise measurements submitted and the machinery should be suitably isolated from the structure using rubber-mounts or other such suitable material to minimise the transference of sound through the structure to neighbouring properties.

External:

None

Relevant Policies

London Plan (2008)

3C.23	Parking Strategy
4B.1	Design Principles
4B.3	Maximising the Potential of Sites
4B.7	Respect Local Context and Communities
4B.8	Respect Local Character and Context
Annex 4	Parking standards.

Unitary Development Plan

(I)GD1	Regard to Surroundings / Integrated into Local Community
(I)GD2	Quality of Life and Visual Amenity
(II)GD3	Character / Design
(II)GD6	Traffic generation
(II)GD8	Site access and servicing
(II)H12	Residential extensions
(I)EN6	Environmental Impact of developments

Local Development Framework - Core Strategy Preferred Options

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. The Core Strategy is at an early stage in its adoption process. As this continues the weight given to it will grow and the relevant objectives are reported to demonstrate the degree to which the proposals are consistent with the emerging policy direction.

- SO1 Sustainability and Climate Change
- SO3 Protect and enhance Enfield's environmental quality;
- SO7 Distinctive, balanced, and healthier communities
- SO11 Safer and stronger communities
- SO16 Preserve the local distinctiveness
- SO17 Safeguard established communities and the quality of the local environment

Other Material Considerations

- PPS1 Delivering Sustainable Communities
- PPG13 Transport

Analysis

Two Storey Side Extension

The proposed two storey side extension at 3.6 metres wide would infill the space to the side of the property up to the side boundary which is common with the rear boundary of Nos 6, 8 and 10 Radcliffe Road. Visually, the extension is considered to be acceptable in terms of its size and appearance within the context of the street scene and is typical of many such extensions. Moreover, negotiations have secured improvements to its appearance through an increase in the height of the roof over the side extension. In addition, due to the juxtaposition of the application property to those neighbouring it, there is no requirement for an inset at first floor level to address terracing.

In terms of the impact on neighbouring properties, although the flank wall would be sited on the boundary, the extension would be sited in the region of 18m from the rear of the properties at Nos. 6, 8 and 10 Radcliffe Road. Consequently, it is considered the relationship would not give rise to conditions prejudicial to the outlook and amenities of these properties.

Concerns have been raised regarding the impact of the extension and in particular, the excavation in connection with the basement, on trees located in the neighbouring rear gardens. Whilst the loss of significant trees having a wider public amenity would not be supported, the conifers do not have this wider public quality and are not worthy of protection by way of a TPO. Moreover, it is considered that only those closest to the boundary may be affected and although they do have local amenity value, on balance, it is considered that the potential effects are not sufficient to warrant refusal of the application.

It should also be noted that there is no requirement for the extension to be sited in from the side boundary to enable future maintenance.

Basement Garage and Car Lift

Located beneath the proposed two storey side extension, this basement garage would provide additional off street parking accessed via a mechanical lift. This element would have no external presence and is therefore acceptable. In addition, Transportation have confirmed that they have no objection to the lift as the basement parking is extra capacity not required to meet any off street parking need given the existing driveway which would still be available should the lift fail.

A significant level of objection though has been received about this element of the proposal from the occupiers of neighbouring properties. In particular, reference is made to noise and vibrations emanating from the operation of the car lift and information obtained from other Local Planning Authorities which have had applications for car lifts. In response additional information has been submitted by the Applicant regarding the noise levels to address these concerns.

Having reviewed this information, Environmental Health have confirmed that if the car lift operates at it designed parameters, the noise levels due to its operation should not be a problem. Furthermore, it is noted that the machinery should be suitably isolated from the structure using rubber-mounts or other such suitable material to minimise the transference of sound through the structure to neighbouring properties.

Accordingly, notwithstanding the concern of residents, it is considered that with the imposition of conditions covering the points raised above, the proposed basement garage and car lift would not give rise to levels of noise which would harm the amenities of neighbouring and nearby residential properties.

Having regard to the vibrations, a similar hydraulic car lift was installed in 2007 in a property in Cockfosters Road. This is a detached property, with the car lift being installed on the right hand side of the property. The neighbouring detached property is immediately adjacent so the relationship of that car lift to the neighbouring property is closer than that proposed here. The car lift on Cockfosters Road has been installed successfully to meet Building Regulations, and has not caused an adverse impact on the neighbouring property as regards vibrations. This is therefore considered a precedent within the Borough for such a development, and as such the issue of vibrations is considered acceptable.

Single Story Rear Extension (Conservatory)

Sited to the rear of the two storey rear extension, this would project 4.5 metres beyond the rear elevation. Amendments have secured reductions to the height of the glazed roof and taking this into account, the relationship to the neighbouring properties at Nos 6, 8 and 10 Radcliffe Road, this element is considered acceptable.

Conclusion

It is considered that the proposed development is acceptable having regard to the applicable policy and thus, it is recommended that this application be granted for the following reasons:

1. The development makes appropriate provision for access and car parking and would not give rise to unacceptable on street parking conditions prejudicial to the free flow and safety of traffic on the adjoining highways, having regard to Policies (II)GD6 and (II)GD8 as well as Policy 3C.23 of the London Plan and PPG13.
2. The proposed extensions to the property, due to their size and siting do not affect the amenities of the surrounding residential properties through a loss of light or outlook or the character and appearance of the surrounding area having regard to Policies (I)GD1, (I)GD2, (II)GD3 and (II)H12 of the Unitary Development Plan and Policy 4B.8 of the London Plan.

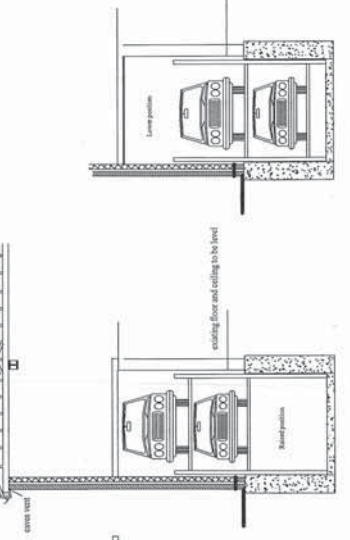
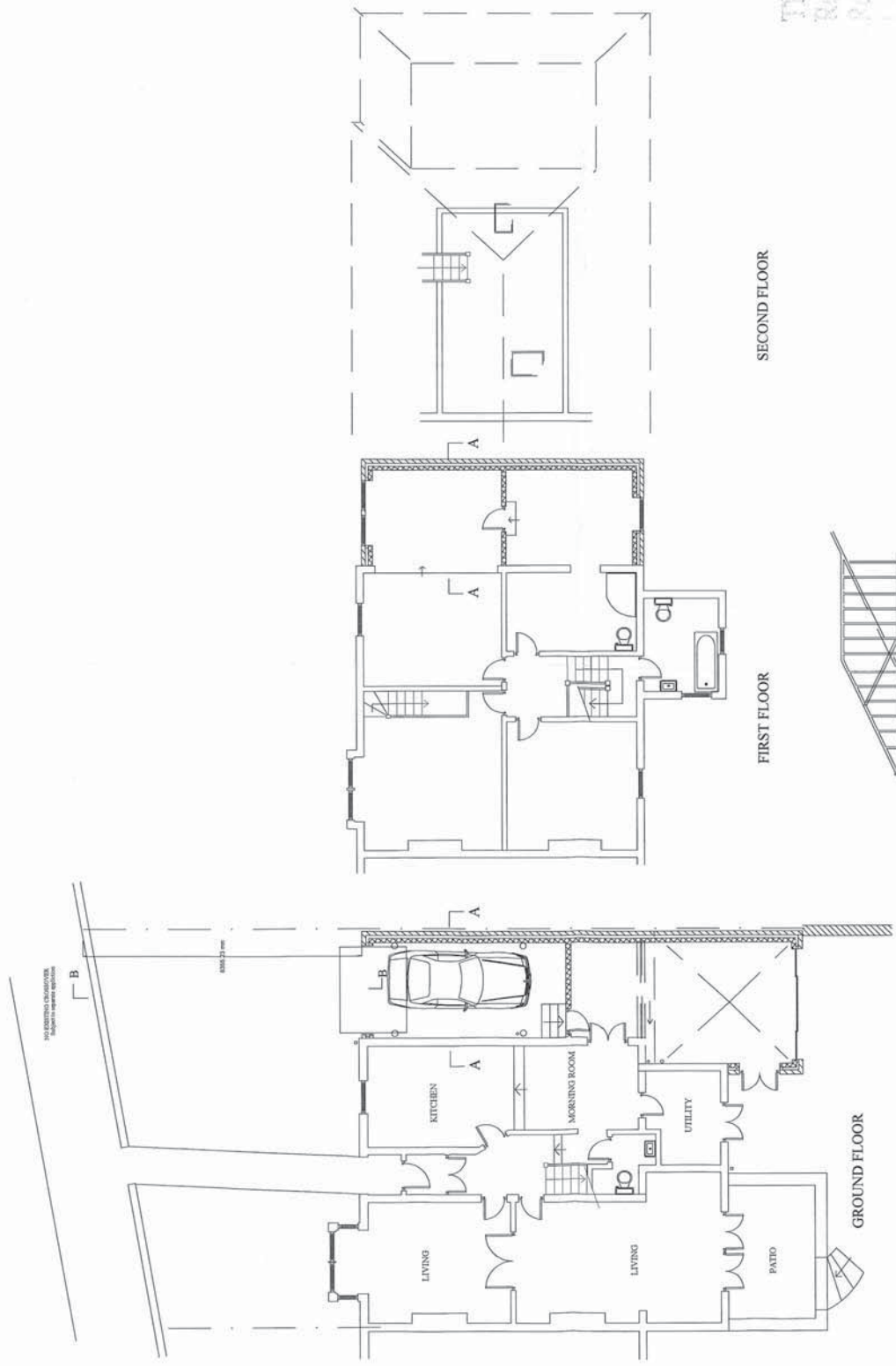
3. The proposed car lift due to the noise specification measures that are included in the installation, would not unduly affect the amenities of the adjacent residential properties through an increase in noise and vibrations having regard to Policies (I)GD1, (I)GD2, (II)GD3, (I)EN6 and (II)EN30 of the Unitary Development Plan.

AP/09/0669

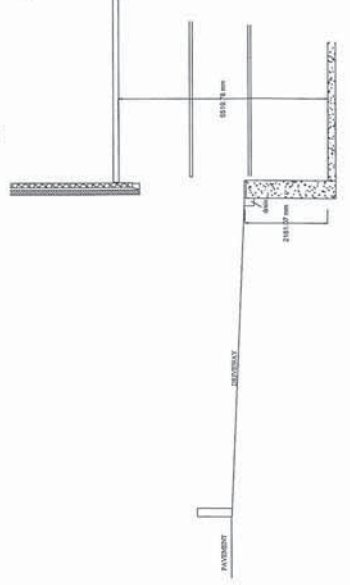
TP 09/0669
 Revised/Additional Drawing
 Received: 3/8/09
 Approved: Agents Letter
 Date: 30/7/09

LONDON BOROUGH OF ENFIELD
 03 AUG 2009
 DEVELOPMENT SERVICES

A. All 1/2 Section BB, and all height measured	
4. Ruckwilt Road London, N21 2SE	Proposed Floor Plans & Sections
PMAS Ltd Proposed Floor Plans & Sections Services Ltd London, N21 2SE Tel: 020 8445 1111 Fax: 020 8445 1112 Date: Feb 2009 Drawing No: 20772A	
Scale: 1:100	

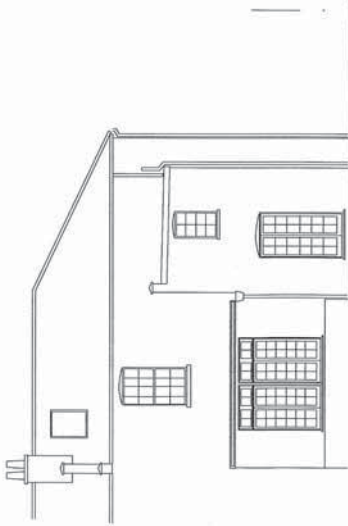


SECTION A A

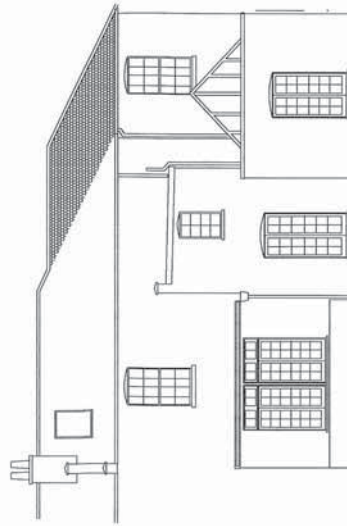


SECTION B B

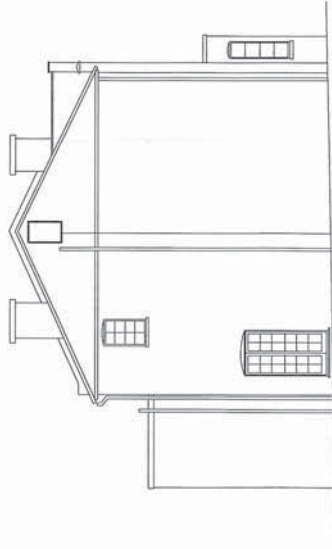
R/09/0669



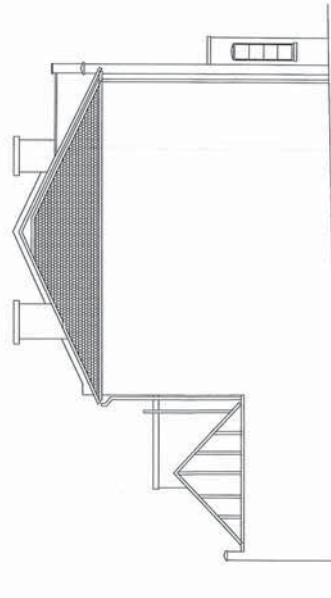
EXISTING REAR ELEVATION



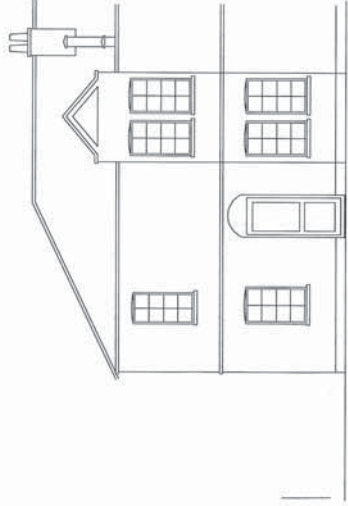
PROPOSED REAR ELEVATION



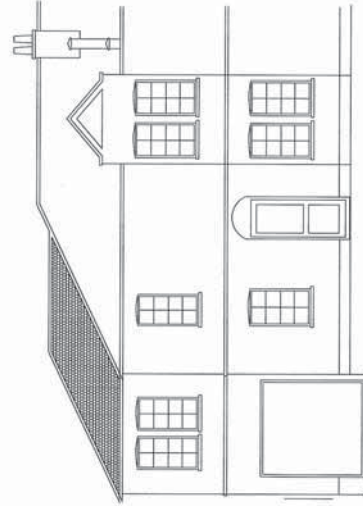
EXISTING SIDE ELEVATION



PROPOSED SIDE ELEVATION



EXISTING FRONT ELEVATION

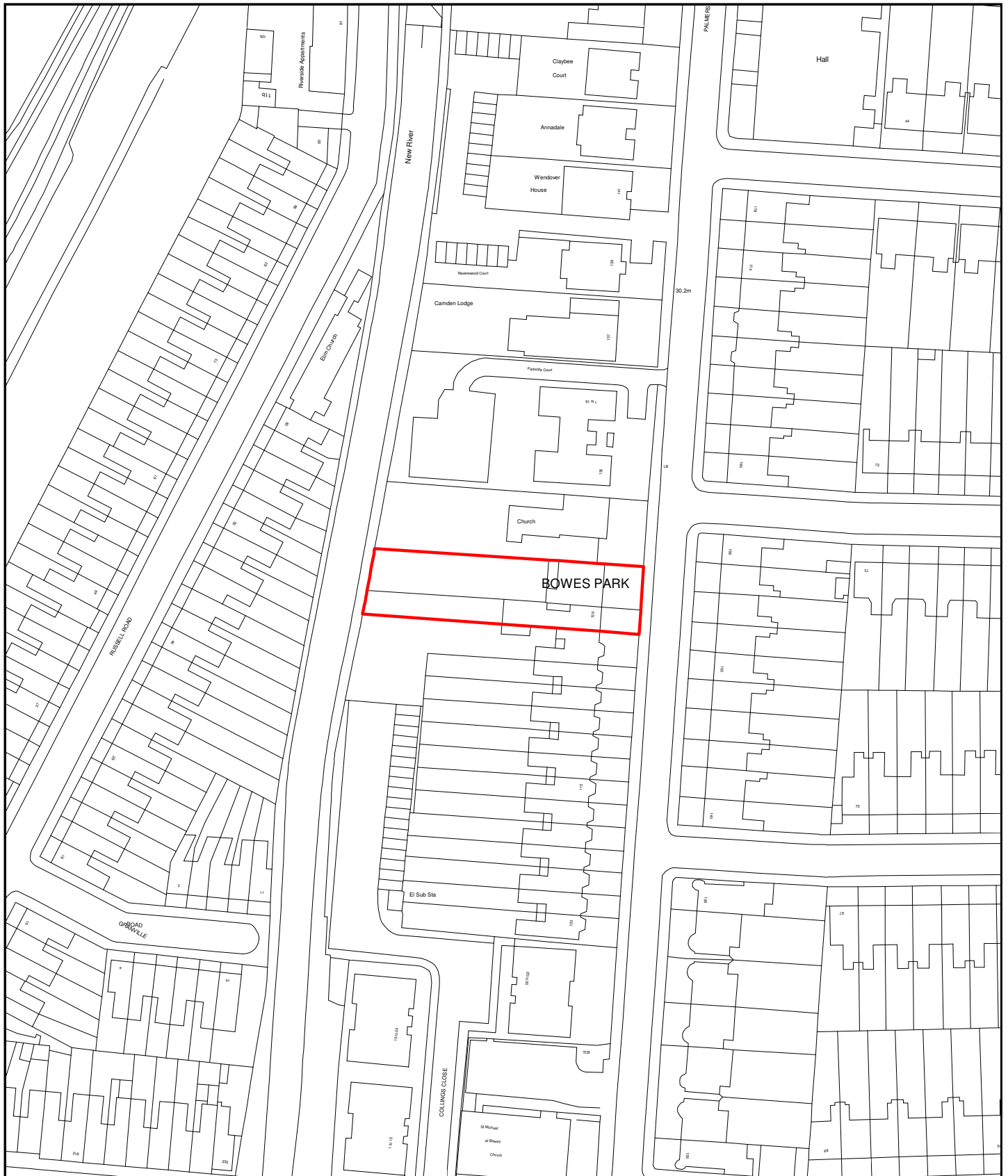


PROPOSED FRONT ELEVATION

TP 09/0669
Revised/Additional Drawing
Received. 3.18.09
Under cover of Agents Letter
..... Dated. 30.7.09



A 2009 City of London Council Development Control	
4, Park Lane, Wood London N21 2NU	Existing & Proposed Elevations
PMSJ CJF Planning Department & Surveying Services Ltd London N11 1UE Tel: 020 2609 1000 Fax: 020 2609 1001 www.pmsj.com	
207771/A Scale 1 : 100	



Development Control



Scale - 1:1250
Time of plot: 15:37

Date of plot: 01/12/2009

Application Number: TP/09/0694 **Ward:** Bowes

Date of Registration: 29th May 2009

Contact: David Warden 3931

Location: 129, PALMERSTON ROAD, LONDON, N22 8QX

Proposal: Redevelopment of site and erection of a part 3, part 4-storey building to provide 9 self contained flats (8 x 2-bed and 1 x 3-bed) with off street parking at rear.

Applicant Name & Address:

Mr Fezil Davut
129, PALMERSTON ROAD
LONDON
N22 8QX

Agent Name & Address:

Mr Ilker Kilich, Kilich & Co.
1, York House
Parkgate Crescent
Hadley Wood
Herts
EN4 0NW

Recommendation: That planning permission be **REFUSED** for the following reason:

1. The proposal due to the excessive depth of the two storey rear projection and the primary ground floor bedroom and ground and first floor kitchen north facing windows would unacceptably impact any future occupant of this property in respect of a loss of outlook and heightened sense of enclosure and a loss of privacy, as well as resulting in the potential curtailment of the adjacent site contrary to (I)GD1, (I)GD2 (II)GD1, (II)GD3 and (II)H8 of the Unitary Development.
2. The proposed development by reason of the lack of sufficient family sized (3 bed+) units would not provide an adequately address the current housing needs of the Borough, nor would it provide a sufficiently balanced community, contrary to Policy (II)H6 of the Unitary Development Plan and Policies 3A.5 and 3A.6 of the London Plan (2008), as well as the objectives of PPS1 and PPS3.
3. Insufficient information has been submitted to demonstrate that the proposal would provide sufficient sustainable design, construction and energy reduction measures contrary to policies 4A., 4A.2, 4A.3 and 4A.7 of the London Plan (2008).

Site and Surroundings

The site is currently occupied by a substantial two-storey detached dwelling with feature stone detailing that is, in design terms, paired with the adjacent building. This has most recently been used as a church. The immediate surrounding area is characterised by two-storey terraced dwellings with prominent bay windows. To the north of the site are more recent three-storey blocks of flats.

Proposal

Permission is sought for the demolition of 129 Palmerston Road and the construction of a 3 storey double fronted building with accommodation in the roof and a two storey rear projection with a terrace above. The replacement building retains approximately the same eaves and ridge height as the existing and adjoining buildings. The scheme includes twin projecting bays and seeking to reflect the style of the adjoining terrace, a range London stock brickwork, off white render and slate style roof covering.

The site will be accessed from Palmerston Road, utilising the existing in and out accesses. Access will be provided to the rear of the site along the boundary with 127 Palmerston Road, where communal amenity space, 9 parking and cycle parking spaces are proposed.

Relevant Planning Decisions

PRE/07/0185 Redevelopment to provide a 3-storey block of 12 flats with accommodation in roof space.

PRE/07/0076 Demolition of existing house and ancillary buildings and erection of a 4-storey building to provide 14 self-contained flats.

TP/00/1858 Part demolition of existing side/rear buildings and erection of a part first floor, part 2-storey side and rear extension to provide a self-contained nanny flat and games/play room, refused February 2001 for the following reason:

The proposed extension by reason of its proximity to the flank boundary and neighbouring properties due to its height, bulk and massing, would prejudice the amenities enjoyed by the occupiers of No. 127 Palmerston Road by reason of loss of light and outlook and is contrary to Policies (I)GD1, (II)GD3 and (II)H12 of the Unitary Development Plan.

TP/91/0072 Redevelopment of site by the erection of 9 No. one-bed flats and provision of parking facilities. (Outline), refused June 1991 for the following reasons:

The site is of insufficient size to accommodate a block of nine 1-bedroomed flats which would comply with the Policies adopted by the Local Planning Authority and in the Borough Development Plan and the Draft Unitary Development Plan with regard to floor areas, amenity space and parking provision.

The proposal envisages an unsatisfactory form of overdevelopment which would be out of keeping with the character of the area and provide a poor standard of amenity for the occupiers of the proposed units.

The proposed development would necessitate the coverage of the majority of the area to the rear of the flats with hard- surfaced parking and turning facilities which would result in an alien and intrusive feature in the locality, detrimental to the visual amenities of the area in general and neighbouring occupiers in particular.

TP/90/1266 Change of use from dwellinghouse to hostel for 25 persons, refused in January 1991.

131 Palmerston Road

TP/09/1240 Conversion of church/meeting hall into 6 self contained flats (2 x 1-bed, 3 x 2-bed, 1 x 3-bed) involving a part 2-storey part single storey rear and side extension with

rear dormer windows and off street parking to front and rear, refused in October 2009 for the following reasons:

The applicant has failed to demonstrate that the proposed 2-storey side/rear extension, by reason of its siting on the boundary with flats at number 135 Palmerston Road, its depth and its height, would not have an undue impact on the light received by primary side windows in the neighbouring residential units detrimental to the amenities of the occupiers thereof. The proposal is therefore contrary to policies (I)GD1, (I)GD2 and (II)H12 of the Unitary Development Plan.

The inclusion of 2 primary bedroom windows, which are the only source of light to the rooms they serve, in the ground floor flank elevation of the development facing towards number 129 Palmerston Road would result in the potential curtailment of development at the adjacent site contrary to (I)GD1, (I) GD2 (II) GD1 and (II) GD3 of the Unitary Development.

The application is currently subject to an ongoing appeal.

TP/09/0142 Conversion of church/meeting hall into 6 self contained flats (2 x 1-bed, 3 x 2-bed, 1 x 3-bed) involving a 2-storey rear extension with rear dormer and off street parking to front and rear, refused in April 2009 for the following reasons:

The proposed conversion of the property into 2 x 1-bed, 3 x 2 bed and 1 x 3-bed self contained residential flats having particular regard to the floor space of Flats E2, E3, E4 and E5, and stacking of the units, would result in an over-intensive use of the property and together with the increased activity, general noise and disturbance and additional vehicle movements associated with the proposed level of occupation, would not only detract from the residential character and amenities of the surrounding area and in particular, the amenities of neighbouring residential properties, but also provide an unacceptable standard of residential accommodation for future occupiers. This would be contrary to Policies (I)GD1, (I)GD2, (II)GD3, (II)H16 and Appendix A1.9 of the Unitary Development Plan as well as the Supplementary Planning Guidance on flat conversions, Policy 3A.6 of the London Plan (2008) and the objectives of PPS1 and PPS3.

The proposed two storey rear and side element of the scheme, due to its size, siting, rearward projection and design, will give rise to conditions through a loss of outlook that will harm the residential amenities enjoyed by the occupiers of no. 129 Palmerston Road, contrary to Policies (I)GD1, (I)GD2, (II)GD3 and (II)H12 of the Unitary Development Plan.

Consultations

Public

Consultation letters have been issued to 76 neighbouring properties. 4 letters of objection have been received which raise all or some of the following points:

Character and Appearance

- Loss of a fine Victorian building, which adds considerably to the character of the area
- One of only two remaining original buildings
- Impact from conversions to flats and HMO's
- Overdevelopment and overcrowding
- Increases over previously refused schemes
- Parking area alien and visually intrusive feature
- Loss of family accommodation

- Impact on residential character
- Small units for single or double occupancy
- Poor design of modern adjoining flats to the north replicated
- Proposal would not respect the appearance of the adjacent buildings, would damage the visual amenity of the street and contribute to the deterioration in the character of the immediate townscape
- Loss of trees
- Refuse from multi-occupancy properties contributes to litter problems

Impact on Neighbours

- Noise and disturbance
- Impact from rear parking area and access
- Combined impact of garage court to the rear of the adjoining terraced dwellings
- Loss of enclosure to the adjoining courtyard garden
- Security of adjoining properties
- Loss of privacy

Highways

- Lack of parking
- Existing conversions place pressure on parking
- 1 parking space but likely to be 2 cars per flat

Other matters

- Impact on wildlife
- Disruption during construction, combined with other works nearby
- Party wall matters
- Impact on adjoining foundations

The Agent's acting for the owners of no. 131 Palmerston Road have written to advise that, subject to obtaining planning permission, the adjoining former Church will be used for residential purposes and the impact on this likely future residential use should be considered. The response goes on to raise concerns regarding the scale of development and the number of proposed dwellings impact upon amenity, use and rights of light of the adjoining property. Furthermore, the response states that the demolition of the existing period structure would be detrimental to the character of the locality and its retention should be sought.

David Burrows MP objects to the application expressing concerns relating to the history of previous refusals at the site, the number of potential residents, the impact on the surrounding area, increased noise and disturbance, lack of parking, disruption during construction, impact on adjoining residents and the security to neighbouring properties.

Councillor Georgiou has also expressed concern that the application would involve the loss of a family home, that the development would be out of character and that the limited parking facilities would mean increased parking on an already congested road.

The Bowes Park Community Association strongly objects to the proposal stating that the existing building is a very fine, unspoilt late Victorian building and garden. It is one of a pair of buildings of such quality that they should really be listed. The group acknowledges that previous similar buildings must have been demolished to construct the flats to the north, but would like to think that there is greater sensitivity today about high quality historic buildings that give an area its character and sense of place. It is suggested that alternative uses for the existing building, including flats or offices are sought. The response goes on to state that the plans are of poor

quality, provide an over development of the site and do not provide the family housing that is needed in the area.

External

Any other responses will be reported at the meeting.

Internal

Any response from the Director of Education, Child Services and Leisure will be reported at the meeting.

Relevant Policies

London Plan (2008)

3A.1	Increasing Supply of Housing
3A.2	Borough Housing Targets
3A.3	Maximising the potential of sites
3A.5	Housing choice
3A.6	Quality of new housing provision
3C.21	Improving Conditions for Walking
3C.22	Improving Conditions for Cycling
3C.23	Parking Strategy
4A.3	Sustainable Design and Construction
4A.20	Reducing noise and enhancing soundscapes
Annex 4	Parking standards

Unitary Development Plan

(I)GD1	Regard to Surroundings / Integrated into Local Community
(I)GD2	Quality of Life and Visual Amenity
(II)GD3	Character / Design
(II)GD6	Traffic Generation
(II)GD8	Site Access and Servicing
(II)H6	Range of size and Tenure
(II)H8	Privacy and Overlooking
(II)H9	Amenity Space
(II)H16	Flat Conversions
(II)T13	Creation or improvement of accesses
(II)T16	Adequate access for pedestrians and disabled persons

Local Development Framework Core Strategy Preferred Options

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. The Core Strategy is at an early stage in its adoption process. As this continues the weight given to it will grow and the relevant objectives are reported to demonstrate the degree to which the proposals are consistent with the emerging policy direction.

SO1	Sustainability and Climate Change
SO3	Protect and enhance Enfield's environmental quality;

SO6	High quality, sustainably constructed, new homes to meet the local people	aspirations of
SO8	Affordable Housing, Family Homes and Social Mix	
SO11	Safer and stronger communities	
SO16	Preserve the local distinctiveness	
SO17	Safeguard established communities and the quality of the local environment	
SO18	Conservation, Listed Buildings and Heritage	
SO21	Sustainable Transport	
CP1	Sustainable and Efficient Land Use	
CP2	Sustainable Design and Construction	
CP5	Air, Water, Noise and Light Pollution and Contaminated Land	
CP10	Managing the Supply and Location of New Housing	
CP12	Housing Mix	
CP14	Safer and Stronger communities	
CP29	Promoting sustainable transport and improving access for people with restricted mobility	
CP31	Walking and Cycling	

Other Material Considerations

PPS1	Delivering Sustainable Communities
PPS3	Housing
PPG13	Transport

Analysis

Principle

The site is currently used as a residential dwelling. Continued residential use of the site would be acceptable and consistent with the character and composition of the surrounding area. In addition, the provision of 9 residential units would increase the supply of housing stock in the Borough in accordance with Policies 3A.1 and 3A.2 of the London Plan. However, the proposal would involve both the loss of a single family dwelling and the demolition of a building that adds significantly to the character of the area.

In respect of the loss of a single family dwelling, the surrounding area, and this street in particular, have been placed under significant pressure from, amongst other things, the conversion of houses into flats. As a result there is a general presumption against the loss of further single family dwellings in this street. However, in this particular case, regard must be had to the size of the existing property and the suitability of single family occupation. Having particular regard to the limited potential for future single family occupation, it is considered, on balance, that it would not be appropriate to seek to resist the application on this basis.

Having regard to all of the above matters and subject to the consideration of the matters detailed below, on balance, the principle of development is considered acceptable.

Loss of Existing Property

This is a substantial and attractive Victorian property that, in conjunction with no. 131 Palmerston Road, makes a significant positive contribution to the character of the area. However, the property is not listed or within a Conservation Area. Having regard to the lack of statutory protection afforded to the building therefore, it is considered that it would not be appropriate to

seek to resist the application on this basis subject to the attainment of a high standard of design in an replacement

Character and Appearance of the area

Density

The site is in PTAL 2 and lies within an area of flats and houses of two and three storeys. Whilst the site is not within 800 metres of a town centre or along a main arterial route, the prevailing character and proximity to the north circular road provide strong urban influences. It is considered, on balance, for the purposes of the London Plan that the site falls within an urban area but with some suburban influences. The matrix suggests a density of 200 to 450 habitable rooms per hectare or a unit range of 45 to 120 units per hectare although in the light of the character of the locality, a density towards the lower end of this range would seem appropriate.

The application proposes 9 residential units comprising 8 two bed and 1 three bed flats resulting in 28 habitable rooms. This would give a residential density of 267 hrph or 86 u/h, which lies towards the lower end of the range. However, advice contained in PPS1 and PPS3, states that a numerical assessment of density must not be the sole test of acceptability and must also depend on the attainment of appropriate scale and design relative to character and appearance of the surrounding area.

In this instance, whilst the front of the proposed block will follow the surrounding pattern of development, the proposed block is of considerable depth. However, there are several large three storey blocks of flats located to the north and within the wider vicinity of the site. These blocks often extend far deeper into the site than the terraced houses to the south. Consequentially, it is considered that such depth would not be out of character with the surrounding form of development. The impact of this depth on the surrounding properties is considered in more detail below.

Having regard to these matters, the extent of site coverage, the numerical assessment detailed above and, in particular, the surrounding pattern of development, it is considered that the proposed density is acceptable and would not result in an overdevelopment of the site.

Amenity space provision

The proposed amenity space provision for the site is made up of communal amenity space to the rear of the proposed block and terraces above the two storey rear projection. The external space and terrace total approximately 331 and 53 square metres, respectively. This provides a total amenity space of approximately 384 square metres, where 14 % is provided in the form of terraces.

The UDP standard requires amenity space to be equal to at least 75% of the flats with two or more bedrooms. Balconies may provide an alternative form of amenity space provided that they are not detrimental to the privacy of adjoining occupiers and do not exceed 15% of the total amenity space provision.

The collective GIA of the units is 557 square metres, resulting in an amenity space requirement of 418: a shortfall of 34 square metres. However, having regard to the flexible approach to planning standards suggested by PPS1 and PPS3, as well as the overall space surrounding the building, it is not considered this small shortfall would warrant the refusal of the application and that the level of amenity space is suitable to meet the needs of future occupiers.

Design and Impact on the Street Scene

The loss of the existing dwelling has been addressed above, which states that due to the loss of the character building a particularly high standard of design is required.

The proposed design seeks to draw upon the characteristics of the building to be replaced, the adjacent building at no. 131 Palmerston Road and the existing terraced properties to the south. It provides a consistent eaves and ridge line with these properties. It also includes bay features with roofs that seek to reflect those prominent on the adjacent terraced properties and stone detailing around the door to reflect no. 131 Palmerston Road. Overall, it is considered that with the selection of the right materials the front elevation would be of the right standard and fully integrate the development into the street scene.

In respect of the side and rear elevations, these are more functional in appearance with, in the case of the side elevations, large areas of unrelieved brickwork, a false pitch over the rear projection and a large central rear dormer. However, due to the alignment of the neighbouring buildings, these elements would not be prominent from public vantage points and would only be seen in part from adjoining properties. As such, it is considered that these matters would not result in visual harm sufficient to warrant the refusal of this application.

Impact on Neighbouring Properties

The proposed two storey element of the building, which includes a glazed screen at roof level, extends some approximately 11.5 metres beyond the rear of no. 127 Palmerston Road, with the three storey element extending approximately 4.4 metres. Furthermore, these elements would be 8 and 9 metres, respectively, from the nearest first floor rear window of no. 127. As a result, the three storey element would not breach a 30 degree line from the rear of the property. However, the two storey element would breach a 30 degree line by some 6 metres but as there is an existing building with a two storey eaves height sited on the boundary with no. 127 that projects approximately some 14 metres beyond the rear of this property and breaches a 30 degree line by some 11.5 metres, the proposal would not result in a significant reduction in this impact on the outlook from the adjoining property. It is considered this depth is therefore acceptable. There are no windows located in the elevations that would directly face no. 127 and the proposed rear facing windows would not lead to overlooking that would be unreasonable.

The proposed vehicle access would run along the boundary with no. 127 to serve the parking area which would also abut the boundary. In addition, the cycle storage is located in this area. However, the applicant proposes a 2 metre high acoustic wall to provide screening from additional noise and as a result, it is considered, on balance, that the impact on residential amenity arising from these elements would not unduly affect the amenities of the neighbouring property.

The adjoining occupier at no. 127, has expressed concern regarding the removal of the existing wall that encloses the area to the rear garden of this property. A request has been made that the wall should be retained at a height of 4 metres. Whilst this would impact on the outlook of this property, it does reflect the existing situation where the wall provides additional privacy and is heavily planted. It is considered that this is a matter that can be adequately addressed by condition.

In respect of no. 131 Palmerston Road, the two storey rear projection and the three storey main element would project beyond the rear of this building by approximately 8.8 and 1.8 metres, respectively, at a distance of approximately 7 and 6 metres from the nearest window, respectively. Whilst the three storey element would not breach a 30 degree line from this window, the two storey element would breach such a line by some 4.3 metres. Unlike the

boundary with no. 127, there is not existing building in this location. Whilst no. 131 was most recently used as a place of worship, the planning history section above details applications for conversion to flats which have been refused for reasons other than the principle of development. As a result, it is considered it would be unreasonable to allow development that would have such a significant impact on this property and curtail the development potential of this adjoining site in favour of the application site. In addition, the scheme includes ground floor bedroom and ground and first floor kitchen windows that would directly overlook no. 131 at a distance of between only 1 and 2 metres from the site boundary. Overall, it is considered the impact on this adjoining property and curtailment of its potential development would be unreasonable.

The proposal also includes velux roof lights that would face towards both no.'s 127 and 131. However, these each provide secondary windows to rooms that also benefit from front or rear velux roof lights. As a result, these side facing roof lights could be obscured and fixed by condition to address this concern.

Overall, it is considered, on balance, that whilst the impact of the proposal on no. 127 is acceptable, for the reasons outlined above, the impact on no. 131 is not.

Parking and Access

The site is located within PTAL 2, an area of low public transport accessibility. The proposal includes 100% parking and cycle provision for the flats, which is considered acceptable. The impact of the access and parking area on adjoining residents is discussed above. As submitted the access to the car parking area at the rear would require one way working along a distance of some 40 metres. However, it is considered that this could be reduced to approximately 20 metres by increasing the width of the access to the rear of the site. This could be secured by condition. Whilst this distance may still require some vehicles to make reversing manoeuvres, the in and out access to the front of the site would provide the opportunity for vehicles to wait without reversing into the highway. Having regard to the limited number of vehicles movements, the reduce one way working and the space to the front of the building to manoeuvre, the proposed access is considered acceptable.

The proposed parking area provides adequate manoeuvring space. Whilst the proposed refuse storage is located to the front of the blocks within adequate distance of the highway for collection, the proximity of the southern bin stores to the highway boundary would impact on pedestrian visibility. However, this could be addressed by condition. As such, it will be necessary to secure details of levels, enclosure, refuse storage, cycle security and a construction management plan by condition.

Concerns have been raised regarding the increased volume of traffic to Palmerston Road. The locality is currently subject to the implementation of an A406 related area based scheme, to help manage traffic movements/create 1-way working, and increase parking controls'; & add at the end of the Para 'Nevertheless there are still concerns about traffic/traffic speeds and the works referred to above do not address this, so it is requested that the development contributes £10k towards speed activated warning sign. However, whilst it is acknowledged that the road is heavily trafficked, it is not considered that the proposed development would result in an increased in vehicle movements to an extent that would warrant the refusal of planning permission.

Overall, subject to the matters to be addressed by condition discussed above, the parking and access arrangements for the site are considered acceptable.

Housing Mix

The scheme includes predominantly two bedroom units, but is located within an area where family housing and larger units would be appropriate. The current Housing Needs Assessment seeks for at least 50% of all new dwellings to provide at least 3 bedrooms. Having regard to the sites location away from any of the Borough's main town centres, it is considered that the scheme should more closely address the need for family sized accommodation. In addition, this would seek to mitigate the loss of a single family unit as discussed above. It is therefore considered that the proposal does not include an adequate mix of units that would address the current housing needs of the Borough due to a lack of sufficient 3 bed+ units and that this should form a reason for refusal.

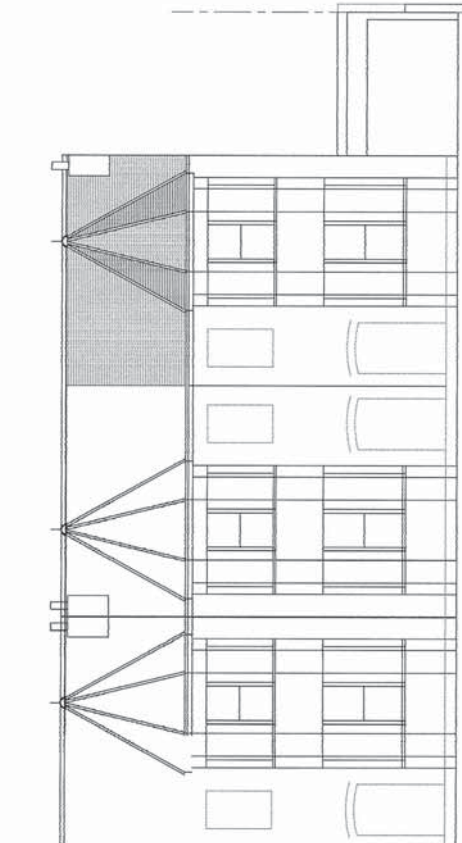
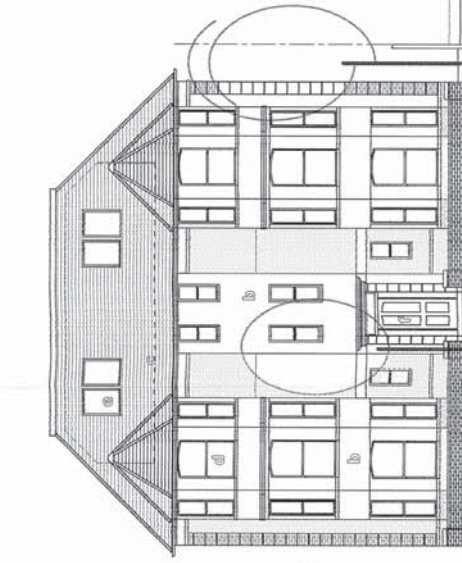
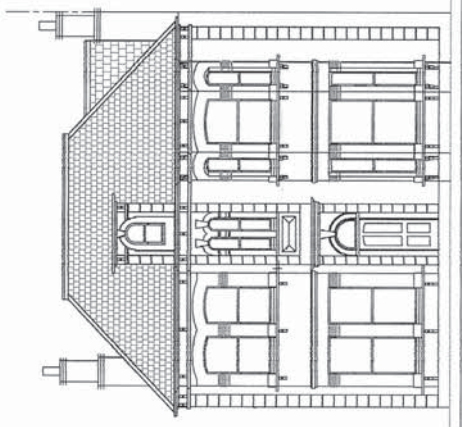
Sustainable Design and Construction

The application includes very limited details of sustainability measures within the scheme. Whilst it is acknowledged that due to the scale of development a full energy assessment is not required, the application lacks any illustration of how the development will seek to address the sustainable design and construction and energy reduction targets of the London Plan. Due to the lack of information it would not be possible to adequately address this matter by condition. Accordingly, a reason for refusal is proposed due to the lack of information relating to sustainable design and construction measures.

Conclusion

In the light of the above assessment, it is considered that the proposed be refused on grounds relating to the impact of the building on no. 131 Palmerston Road, housing mix and sustainable design and construction. However, there is no objection to the principle of development or the loss of the existing building.

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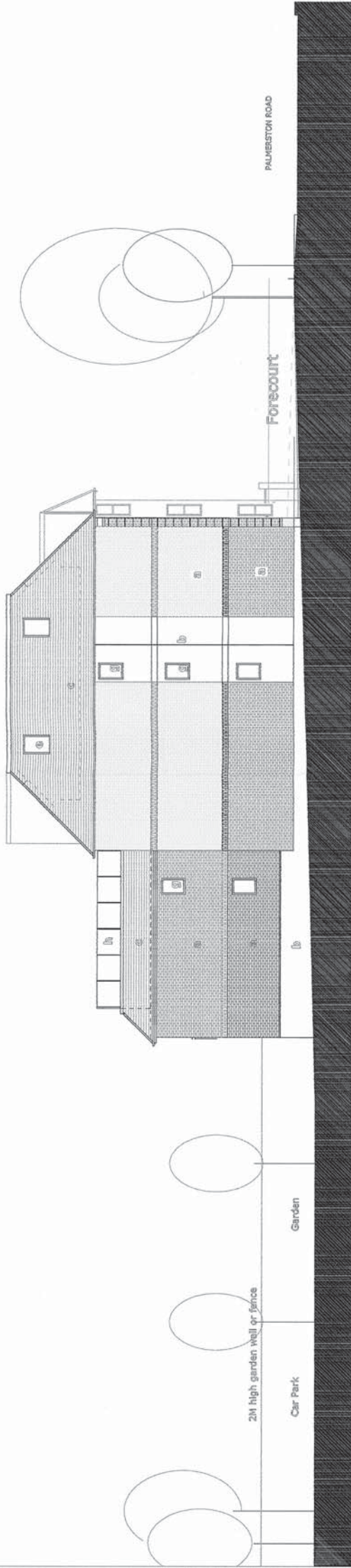
127

- a. EXTERNAL WALLS - Facing Brickwork (London stock) and self coloured render
- b. EXTERNAL WALLS - Self colour render in off white
- c. ROOF - Emit charcoal grey / slates
- d. WINDOWS & DOORS - Double glazed with white frame
- e. ROOF WINDOWS - "Velux" roof windows - suitable for means of escape
- f. ENTRANCE - Panelled timber door with feature stone surrounds

131

JINDON BOROUGH OF ENFIELD
DEVELOPMENT SERVICES
14 MAY 2009

Killich & Co Architects Ltd, 200 Palmerston Road, London N22 One Palmarston Crescent, Haver Wood, Herts SG12 0WY	
PROJECT PROPOSED RE-DEVELOPMENT AT 129 PALMERSTON ROAD N22	
CLIENT FEZIL DAVUT 129 Palmerston Road London N22	
DRAWING FRONT (east) ELEVATION	
Drawn by	date
HLK	June07
scale	1 : 100
DRAWING NO. 129PR-06A	



- a. EXTERNAL WALLS - Facing Brickwork (London stock) and self coloured render
- b. EXTERNAL WALLS - Self colour render in off white
- c. ROOF - Eternit charcoal grey / slates
- d. WINDOWS & DOORS - Double glazed with white frame
- e. ROOF WINDOWS - "Velux" roof windows - suitable for means of escape
- f. ENTRANCE - Panelled timber door with feature stone surrounds
- g. BATHROOM WINDOWS - Fixed window with obscured glass
- h. GLASS SCREEN - Frosted safety glass screen around roof terrace

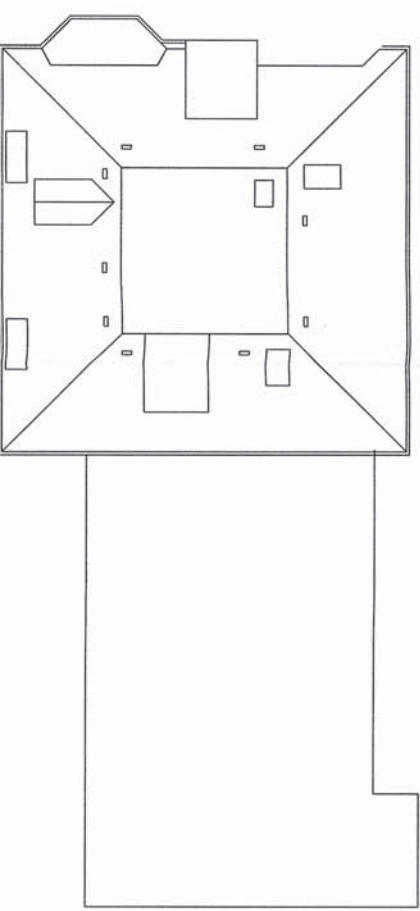
Kilich & Co		Architects and Interiors	
One Kingsgate Crescent Newley Wood HATFE BDA 20W			
Project			
PROPOSED RE-DEVELOPMENT AT			
129 PALMERSTON ROAD NZZ			
client			
FEZIL DAYUT			
129 Palmerston Road London N22			
dwg. title			
SIDE (south) ELEVATION			
dwg. by	date	scale	
lak	June07	1 : 100	
dwg. no.			129PR-08A

129PR01B

131

129

127



PALMERSTON ROAD
16,380 M

Kilich + Co
architectural and development consultants
One Parkgate Crescent Holey Wood HERTS EN NW
Project
PROPOSED RE-DEVELOPMENT AT
129 PALMERSTON ROAD N22
client
FEZIL DAVUT
129 Palmerston Road London N22
dwg title
GROUND FLOOR PLAN
dwg by
lak
date
June07
scale
1 : 100
dwg.no.
129PR-01B

02 JUN 2008
LONDON REGION OF ENFIELD
DEVELOPMENT SERVICES

widened to 3100
widened to 3100

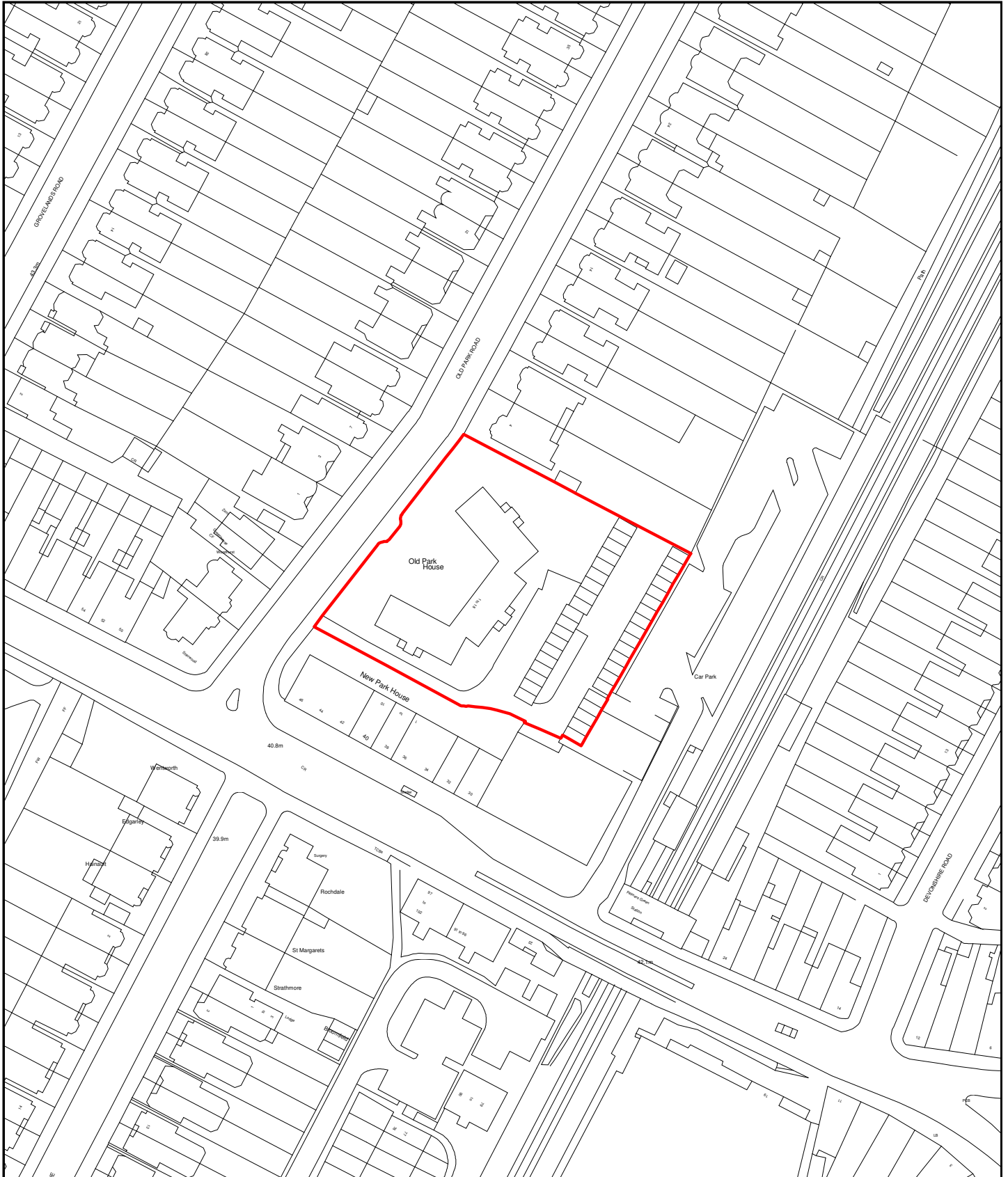
5 bin stores for refuse and re-cycling
4 bin stores for refuse and re-cycling

1000
2000
3100

2M HIGH BOUNDARY CN/FENCE
2M HIGH ACOUSTIC WALL
in 215 brickwall or specialist's acoustic wall

garden 223
cycle parking 9-11 spaces

MAIN ENTRANCE



Development Control



Scale - 1:1250
Time of plot: 16:08

Date of plot: 01/12/2009

Application Number: TP/09/1091 **Ward:** Winchmore Hill

Date of Registration: 23rd July 2009

Contact: David Warden 3931

Location: 1-18, OLD PARK HOUSE, OLD PARK ROAD, LONDON, N13 4RD

Proposal: Demolition of garages at rear and erection of a 3-storey block to provide 9 self contained flats (comprising 8 x 2-bed and 1 x 1-bed) together with a new parking area to provide 27 parking spaces.

Applicant Name & Address:

Old Park House Company
4, Ebbon Square
Lower Earley
Reading
Berkshire
RG6 4JT

Agent Name & Address:

Mr Phil Waind RIBA ARB ACA, Waind Gohill Architects
27, Bulwer Street
London
W12 8AR

Recommendation: That planning permission be **GRANTED** subject to the following conditions:

1. The development shall not commence until details of the louvers, in particular on the northern elevation, which shall be arranged to minimise overlooking of the adjacent properties, have been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details prior to the occupation of any flat hereby approved and retained thereafter.

Reason: To prevent an unacceptable level of overlooking to the adjoining properties.

2. The development shall not commence until a PPG24 Noise Assessment has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details prior to the occupation of any flat hereby approved and retained thereafter.

Reason: To protect the amenities of future residents.

3. The development hereby approved shall not commence until details of a scheme to provide solar thermal hot water systems for each of the flats to provide at least 10% of onsite renewable energy have been submitted to and approved in writing by the Local Planning Authority. These works shall then be implemented prior to the occupation of any residential unit hereby approved and shall thereafter be retained.

Reason: In the interests of promoting renewable energy and mitigating climate change.

4. The development hereby approved shall not commence until details of a Construction Management Plan have been submitted to and approved in writing by the Local Planning

Authority. The plan shall be implemented in accordance with the approved details before any works are carried out on site and shall remain in place until the development is completed.

Reason: To protect the amenities of neighbouring residents and in the interest of highway safety.

5. The 27 parking spaces shown on approved plan 160_005 received by the Local Planning Authority on 21st July 2009 shall be marked out and numbered prior to the occupation of any residential unit hereby approved. The spaces so provided shall be retained thereafter solely for the parking of private motor vehicles and shall not be used for any other purpose.

Reason: To ensure that the development provides adequate off street parking and to prevent the introduction of activity which would be detrimental to amenity.

6. No development shall take place until an assessment has been carried out into the potential for disposing of surface water by means of a sustainable drainage (SuDS) scheme, in accordance with the principles of sustainable drainage systems set out in national planning policy guidance and statements, and the results of that assessment have been provided to the local planning authority. The assessment shall take into account the design storm period and intensity; methods to delay and control the surface water discharged from the site; and measures to prevent pollution of the receiving groundwater and/or surface waters.

Reason: To ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere.

7. Surface water drainage works shall be carried out in accordance with details that have been submitted to, and approved in writing by, the local planning authority before the development commences. Those details shall include a programme for implementing the works. Where, in the light of the assessment required by the above condition, the local planning authority conclude that a SuDS scheme should be implemented, details of the works shall specify:

- i) a management and maintenance plan, for the lifetime of the development, which shall include the arrangements for adoption by any public authority or statutory undertaker or any other arrangements to secure the operation of the scheme throughout its lifetime; and

- ii) the responsibilities of each party for implementation of the SuDS scheme, together with a timetable for that implementation.

Reason: To ensure implementation and adequate maintenance to ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere.

8. The development hereby approved shall not commence until a scheme for waiting restrictions to protect and improve the visibility splays on both sides of the access to the site has been submitted and approved in writing by the Local Planning Authority. These works shall then be implemented prior to the occupation of the development or otherwise agreed to in writing by the Local Planning Authority.

Reason: In the interests of highway safety and free flow traffic

9. C07 Details of Materials
10. C09 Details of Hard Surfacing
11. C10 Details of Levels
12. C11 Details of Enclosure
13. C14 Details of Access and Junction
14. C17 Details of Landscaping
15. C19 Details of Refuse Storage & Recycling Facilities
16. C25 No additional Fenestration
17. C59 Cycle parking spaces
18. C51A Time Limited Permission

Site and Surroundings

The site lies to the rear of Old Park House: a three storey residential block containing 18 flats, and currently contains 39 lock-up garages, which are accessed from Old Park Road. To the east of the site lies the car park serving Palmers Green station, to the south lies a 3 storey commercial building and the 3 storey mixed retail residential parade of New Park House, whilst to the north is the garden of 4 Old Park Road. The surrounding area is predominantly residential with shops and commercial premises linked to the neighbouring Palmers Green Town Centre and Alderman's Hill local centre.

Proposal

Permission is sought for the demolition of the existing garages and the construction of a three storey block providing 9 flats located towards the northeast corner of the site. The block is visually divided into 3 sections each with a staggered footprint and step down in height as the land falls away from Old Park House towards the station car park. The proposal involves a modern design comprising flat green roofs, glazed and timber elements, along with projecting and recessed panels. The area around the block will provide communal amenity space, in conjunction with the retained amenity space around Old Park House.

The proposal will utilise the existing access to Old Park House. Three parking spaces are provided alongside the access road, with 24 further spaces within a parking courtyard in front of the proposed block. A total of 27 parking spaces are provided for the retained 18 flats at Old Park House and the proposed 9 flats. Refuse storage is provided adjacent to the proposed parking area.

Relevant Planning Decisions

TP/08/1285 Redevelopment of site to provide 10 flats with associated parking (OUTLINE - all matters reserved), refused in October 2008 for the following reasons:

The proposed redevelopment of site to provide 10 flats with associated parking (OUTLINE - all matters reserved), would by reason of the restricted size of the site and siting of surrounding properties represent an over-development of the site that would provide a cramped appearance

detrimental to the character and appearance of the surrounding area and have an unsatisfactory relationship with the adjoining residential properties in terms of overbearing impact and an unacceptable degree of overlooking with a consequential loss of privacy detrimental to the amenities enjoyed by the occupiers of these neighbouring properties. This would be contrary to Policies (I)GD1, (I)GD2, (II)GD3, (II)GD1, (II)GD3, (II)H8 and Appendix A1.7 of the Unitary Development Plan.

Insufficient information has been submitted to demonstrate that the proposed development would make adequate provision for access, servicing and car parking having regard to Policies (II)GD6 and (II)GD8 of the Unitary Development Plan and Policy 3C.23 of the London Plan (2008), as well as guidance contained within PPG 13.

PRE/06/0119 Development at rear of side to provide 15 self contained flats.

TP/04/2265 Residential development (Outline - siting and means of access), refused February 2005 for the following reason:

The proposed outline application for residential development would by reason of its siting and relationship to boundaries and existing buildings represent an over-development of the site that would be unduly cramped and have an unsatisfactory relationship with the adjoining residential properties in terms of overbearing impact and overlooking and consequent loss of privacy detrimental to the amenities enjoyed by the occupiers of these neighbouring properties. This would be contrary to Policies (I)GD1, (II)GD1, (II)H8 and Appendix A1.7 of the Unitary Development Plan.

Public

Consultation letters have been issued to 74 neighbouring properties. Six letters of objection have been received which raise all or some of the following points:

Character and Appearance

- Overly dense development
- Small and cramped site
- Overcrowding of an already highly congested area
- Modern design is out of character with existing houses
- Inadequate units sizes

Impact on Neighbours

- Unsuitable location of waste bins
- Noise and disturbance
- Disruption during construction

Highways

- Loss of garages, resulting in increased kerbside parking
- Concerns whether parking includes provision for existing residents
- Inadequate site access width
- Inadequate access for fire appliances and service vehicles
- Lack of safe pedestrian access
- Lack of turning space for larger vehicles
- Poor access during construction

- Impact on increased use of substandard access on doctors surgery opposite
- Loss of storage for classic cars and for local retailers
- Lack of parking, particularly New Park House and for disabled residents of existing flats, including during construction

Other matters

- Inaccuracies in plans – solar panels do not accord with green roofs and cycle racks not shown
- Incorrect statements - that the site is vacant and that the garages are derelict
- Lack of consultation of those renting garages
- Impact on the foundations of Old Park House

A letter of support from an adjoining resident has also been received commenting positively on the design and consideration shown to the site and adjoining residents. Within this scheme, over that previously proposed, overlooking is reduced, the new access is removed and any dominance of the scheme is removed.

In addition ,Fox Lane & District Residents' Association objects to the application stating concerns regarding the number of conversions and small units within the area, which house an inevitably transient population. The response goes to outline the following concerns:

- Cramped accommodation, compared with that in the existing Old Park House and New Park House, and properties in the surrounding area
- Over-development
- Unsatisfactory as family homes
- Noise and disturbance
- Loss of 39 garages to be replaced by only 27 parking spaces
- Severe parking difficulties in the vicinity
- Nearly all of the garages are in use, some by residents from the local area as well as those from the existing flats in Old Park House and New Park House.
- Inadequate bin store size.
- Inadequate emergency service access
- Disruption during construction

External

The Whitewebbs Museum of Transport and the Enfield & District Veteran Vehicle Trust raises concerns that many classic cars are housed in the existing garages. Without them the owners of these vehicles would have great difficulty in finding alternative accommodation.

Network Rail has no objection subject to conditions relating to drainage, construction management including method statements, foundations, security fencing, noise, landscaping and lighting.

Any other responses will be reported at the meeting.

Internal

Any response from the Director of Education, Child Services and Leisure will be reported at the meeting.

Relevant Policies

London Plan

3A.1	Increasing Supply of Housing
3A.2	Borough Housing Targets
3A.3	Maximising the potential of sites
3A.5	Housing choice
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Unitary Development Plan

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(II)H9	Amenity Space
(II)T13	Creation or improvement of accesses
(II)T16	Adequate access for pedestrians and disabled persons

Local Development Framework Core Strategy Preferred Options

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. The Core Strategy is at an early stage in its adoption process. As this continues the weight given to it will grow and the relevant objectives are reported to demonstrate the degree to which the proposals are consistent with the emerging policy direction.

SO1	Sustainability and Climate Change
SO3	Protect and enhance Enfield's environmental quality;
SO6	High quality, sustainably constructed, new homes to meet the aspirations of local people
SO8	Affordable Housing, Family Homes and Social Mix
SO11	Safer and stronger communities
SO16	Preserve the local distinctiveness
SO17	Safeguard established communities and the quality of the local environment
SO18	Conservation, Listed Buildings and Heritage
SO21	Sustainable Transport
CP1	Sustainable and Efficient Land Use
CP2	Sustainable Design and Construction
CP5	Air, Water, Noise and Light Pollution and Contaminated Land
CP10	Managing the Supply and Location of New Housing
CP12	Housing Mix
CP14	Safer and Stronger communities

CP29	Promoting sustainable transport and improving access for people with restricted mobility
CP31	Walking and Cycling

Other Material Considerations

PPS1	Delivering Sustainable Communities
PPS3	Housing
PPG13	Transport

Analysis

Principle

The site is located in close proximity to Palmers Green Town Centre within a predominantly residential area. The use of the land for residential purposes would be consistent with the surrounding character of the area. It would also increase the supply of housing within the Borough assisting in the attainment of the Boroughs housing targets. The principle of the proposed development therefore, subject to the detailed considerations below, is considered acceptable.

Character and Appearance of the area

Density

The site is adjacent to Palmers Green station and very near to the Town Centre. It lies within an area characterised by mixed-use development including terraced houses and flats of 2-3 storeys, although there are also a considerable number of semi-detached properties. The site lies in PTAL 3 and for the purposes of the London Plan 2008 density matrix it is considered the site lies within an urban area. The matrix suggests a density of 200 to 450 habitable rooms per hectare or a unit range of 45 to 120 units per hectare although given the character of the locality, a density at the lower end of this range would seem appropriate.

The application proposes 9 residential units comprising 1 one bed and 8 two bed flats resulting in 26 proposed habitable rooms. The retained building (Old Park House) includes 18 units with 54 habitable rooms providing a total of 27 units and 80 habitable rooms. This would give a residential density of 75 u/h or 222 hrph which lies towards the lower end of the range. Whilst this excludes the properties at New Park House, (within the control of the applicant) these do fall outside of the application site and have their own curtilage. However, even when taking these 10 flats and their surrounding curtilage into account, the density for the enlarged site would be 79 u/h or 234 hrph. These also lie well within the range set out in the London Plan. However, advice contained in PPS1 and PPS3, states that a numerical assessment of density must not be the sole test of acceptability and must also depend on the attainment of appropriate scale and design relative to character and appearance of the surrounding area.

In this instance, the siting and orientation of the surrounding buildings and the lack of direct street frontage are factors that will limit the scale of development that is acceptable within the site. However, the site is located in close proximity to the Town Centre and, moreover, the proposed building is subordinate to the existing Old and New Park House and is sited to minimise its impact on these and adjoining buildings and maximise surrounding amenity space.

Having regard to these matters, as well as the surrounding patterns of development, the extent of site coverage and the numerical assessment, it is considered that the proposed density is acceptable and would not result in an overdevelopment of the site.

Amenity space provision

The proposed amenity space provision for the site is made up of communal amenity space around the proposed block, as well as that retained around Old Park House, and private balconies to the rear of the first and second floor units. The balconies collectively total approximately 20 square metres and the retained communal and proposed amenity space are approximately 1,633 and 571 square metres, respectively. This provides a total amenity space for the proposed and retained dwellings of approximately 2224 square metres.

The UDP standard requires amenity space to be equal to at least 50% of the Gross Internal Area (GIA) of the proposed one bedroom and 75% of the proposed two bedroom flats. Balconies may provide an alternative form of amenity space provided that they are not detrimental to the privacy of adjoining occupiers. The provision of amenity space in the form of balconies and roof terraces should not exceed 15% of the total amenity space provision.

The collective GIA of the units is 548 square metres, including 50 square metres as a one bedroom unit with the remainder forming the two bedroom units. This results in an amenity space requirement of 380.5 sq.m. The proposed amenity space far exceeds this standard and, moreover, even when considering the properties in Old and New Park House, the amenity space standards are exceeded. As less than 10% of the amenity space will be provided in the form of balconies, it is considered the amenity space proposed is of a sufficiently high quality and regular shape to fulfil its purpose. Overall, the proposed amenity space is therefore considered acceptable.

Design and Impact on the Street Scene

The scheme proposes a contemporary design approach to the site that emphasises the angular shape of the proposed building, whilst using these same angles to limit the impact of the proposal on the adjoining properties. This does not reflect the existing Old and New Park House structures. However, it is not felt necessary to seek to mirror these earlier designs. The applicant states within their submission that both Old and New Park House were modern designs of their time and the current proposals seek to provide an equally modern design for now.

The proposal would not be visible from street, but would be prominent from the station car park, platform and trains themselves. Whilst there are some concerns regarding the lack of active frontage facing this direction, this must be balanced with the fact that this does not provide a true street frontage. It should also be noted that this orientation safeguards future residential amenity for occupiers in terms of the views from the station and noise from passing trains and the potential for overlooking from the car park. As a result, on balance, it is considered that the proposed angled orientation of the building, along with the rear balconies and horizontal timbered louvers to the car park this elevation is considered acceptable. In addition, having regard to the shape of the building and its angled siting it is considered it would provide acceptable separation from the site boundaries.

The proposal includes a strong palette of materials including glazing, timber and brickwork under green roofs which will enhance appearance and provide a high quality development when viewed in the surrounding area. Overall, it is therefore considered that the proposed design and siting of the buildings will provide for a suitable contemporary design solution that will make a positive contribution to the site.

Impact on Neighbouring Properties

The side elevation of the proposal, at its nearest point, would be only approximately 12 metres from primary windows within the south elevation of Old Park House. Whilst this elevation would contain no windows, it would be 8.5 metres high and 13.4 metres wide. To mitigate the impact of the proposed development, the proposed block's siting is slightly offset so that it is not directly in front of these affected windows. In addition, the design of the building would then splay away from Old Park House. It is considered this angled footprint of the building, along with its flat roof design and the use of horizontal timber louvers to articulate this elevation would ensure that the proposal would not have an unacceptable overbearing impact or result in an unacceptable loss of light or outlook to the residential accommodation in Old Park House. However, it will be necessary to require details of the louvers on the northern elevation to be submitted for approval to ensure that there will be no overlooking from this elevation. The orientation of the front and rear windows of the proposed block would be such that they would not directly overlook Old Park House.

The rear windows of the proposed building would be sited approximately 10 metres from the boundary with no. 4 Old Park Road. However, the orientation and design of the building would ensure views were only of the very end of the rear garden to no. 4. In addition, the common boundary with No 4 contains a number of mature trees which assist in minimising the presence and degree of overlooking that would occur.

The front of the building would face the rear of New Park House. However, a separation distance of approximately 42 metres would ensure that there is not an unacceptable level of overlooking.

There are some concerns regarding the siting of parking spaces adjacent to Old Park House. To address this, it is proposed that these spaces are allocated to the adjacent ground floor units to avoid any significant impact on amenities of the occupiers of the adjacent flats.

Concerns have been raised regarding the siting of the bin store. However, this will be alongside an existing bin store serving New Park House. As such, it is considered the amenities of these residents would not be adversely affected.

Overall, it is considered that sufficient separation distances have been provided and the design and layout of the scheme ensure that it will not have an unacceptable impact on the amenities of the adjoining properties.

Parking and Access

The site is located within PTAL 3, adjacent to Palmers Green rail station and close to Palmers Green Town Centre. It therefore has good accessibility to a range of public transport. The proposal includes 100% (1 for 1) parking for the 18 flats in Old Park House and the 9 proposed flats, including two disabled spaces. The proposal does not include any spaces for residents of New Park House. However, there was no requirement for dedicated parking when this block was granted planning permission. Having regard to this decision, as well as the accessibility to public transport, it is considered that the proposed parking is acceptable.

The proposal also involves the loss of 39 lock up garages and the applicant has confirmed that many of these garages are not in use: those still in use provide storage rather than day to day parking spaces with a number used to store classic cars in various stages of restoration. However, the loss of these garages would displace these vehicles to other storage facilities rather than to surrounding streets. Whilst the concerns of residents and the Enfield & District Veteran Vehicle Trust are noted, having regard to the need to provide additional housing and the sites sustainable location, on balance, the loss of these storages facilities would not warrant the refusal of this application.

The site is accessed from Old Park Road by widening the existing access from 3.1 to 4.1 metres wide. This widened access would benefit both existing and proposed residents accessing the site. However, due to the increased use of the access there are concerns regarding visibility splays at the Old Park Road especially as the lack of any parking restrictions and parked cars reduce visibility. However, a condition is proposed requiring a scheme for waiting restrictions to protect and improve the visibility splays on both sides of the access.

The site does not provide turning facilities for refuse or fire appliance vehicles. This is also true of the existing layout where parking spaces are occupied. The location of the bin store is adjacent to the existing bin store for New Park House. Whilst it would be preferable for refuse vehicles to be able to enter and turn within the site, the absence of this arrangement for the existing units on site means that it would be difficult to substantiate a reason for refusal on these grounds. In addition, the proposed improvements to the access width would improve conditions for refuse vehicles reversing into the site. In respect of fire appliance access, it may be necessary for the new units to be provided with sprinkler systems or a fire hydrant. However, this is a matter that will be addressed as part of the building control process and is not material to the determination of the application.

Cycle storage space for the new and existing flat will be provided at a ratio of 1 for 1 in close proximity to the entrance to each block. This provision will be secured by condition and as such is considered acceptable.

Overall, it is considered that the proposal provides for adequate access and parking to ensure that it will not adversely impact upon the free flow of traffic or highway safety on the surrounding roads.

Noise

The site is located adjacent to the railway station and the proposed units are likely to be affected by noise from the railway. This is not an unacceptable or uncommon siting of such building and it is considered that the buildings themselves could be sufficiently insulated against noise. There are some concerns regarding noise affecting the use of the proposed amenity space. However, only part of this space would be closer to the railway than the existing amenity space and the separation provided by the car park would act as a buffer. Furthermore, amenity space in a similar proximity to this line has previously been accepted on other schemes. A condition will, however, be required to ensure a PPG24 Noise Assessment is provided and the requisite glazing and other standards implemented.

Housing Mix

It is of considerable concern that the site provides a limited mixture of dwelling types with no family sized units proposed. However, the limited number of units proposed and the previous refusal of schemes that resulted in larger building sizes have served to limit the potential for the site. Overall, having regard to the acceptability of the remainder of the scheme, as well as its proximity to the Town Centre, on balance, it is considered the proposed development is acceptable in this instance.

Sustainable Design and Construction

The application includes green sedum roofs, solar water heating, rainwater harvesting, water efficient appliances, orientation to maximise sun path, and porous tarmac to hard surfaces. Conditions are proposed requiring details of a Sustainable Drainage System and solar water heating to be submitted to and approved. Overall, it is considered that the proposal is in

accordance with the objectives of policy 4A.3 'Sustainable Design and Construction' of the London Plan.

Conclusion

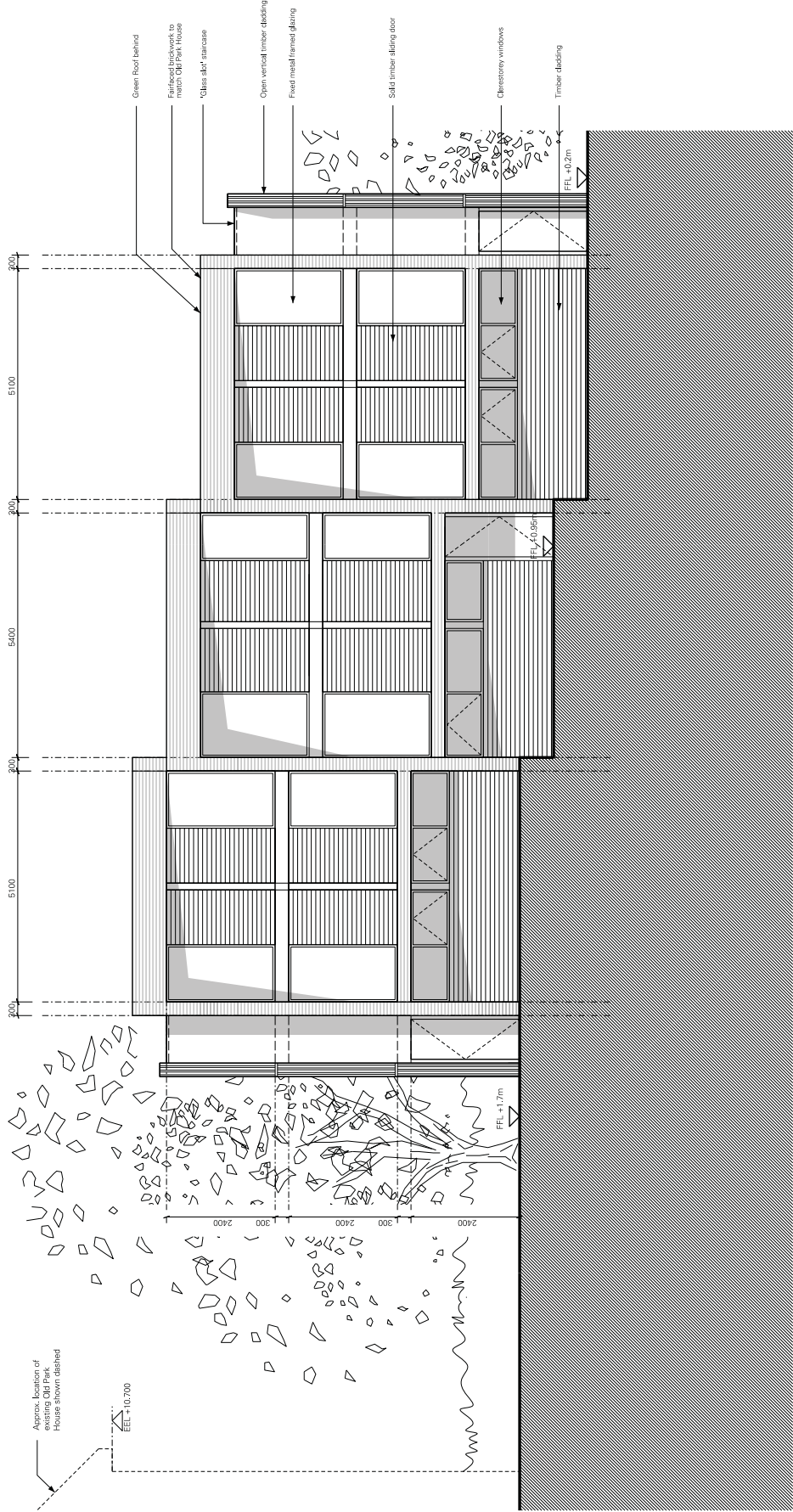
In the light of the above assessment, it is considered that the proposed be approved for the following reasons:

1. The proposed 9 residential units would contribute to increasing the range and quantity of the Borough's housing stock having regard to policies (I)GD1, (I)GD2 and (II)H6 of the Unitary Development Plan, policies 3A.1, 3A.2, 3A.3 and 3A.6 of the London Plan (2008), as well as the objectives of PPS1 and PPS3
2. The proposed 9 residential units would not detract from the character and appearance or the visual amenities of the surrounding area, having regard to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan, as well as the objectives of PPS1 and PPS3.
3. The proposed 9 residential units would not unacceptably affect the amenities of adjoining or nearby residential properties having regard to Policies (I)GD1 and (I)GD2 of the Unitary Development Plan, as well as the objectives of PPS1 and PPS3.
4. The proposed 9 residential units would not prejudice through overlooking or loss of privacy, the amenities enjoyed by neighbouring properties, having regard to Policy (II)H8 of the Unitary Development Plan, as well as the objectives of PPS1 and PPS3.
5. The proposed 9 residential units unit including the provision of 27 parking spaces and 27 secure cycle spaces would not give rise to unacceptable on street parking, congestion or highway safety issues, having regard to Policies (II)GD6, (II)GD8 and (II)T13 as of the Unitary Development Plan, Policy 3C.23 of the London Plan (2008), as well as the objectives of PPG13.

Project	Old Park House, Old Park Road, London, N12 8ED
Drawing	Proposed front elevation
Scale	1:100 @ A3, 1:50 @ A1
Date	May 09
Drawing number	202_102
Revision	A

Revision	Date	Drawn	Approved	Notes
A	01.07.09	JA	PW	Planning

Planning



Proposed front elevation

General Notes

- For proposed location plan refer to 100 005
- For proposed site plan refer to 100 006
- For proposed ground floor plan refer to 100 130
- For proposed first floor plan refer to 100 132
- For proposed 2nd floor plan refer to 100 133
- For proposed roof plan refer to 100 145
- For proposed west elevation refer to 100 146
- For proposed north elevation refer to 100 147
- For proposed south elevation refer to 100 148

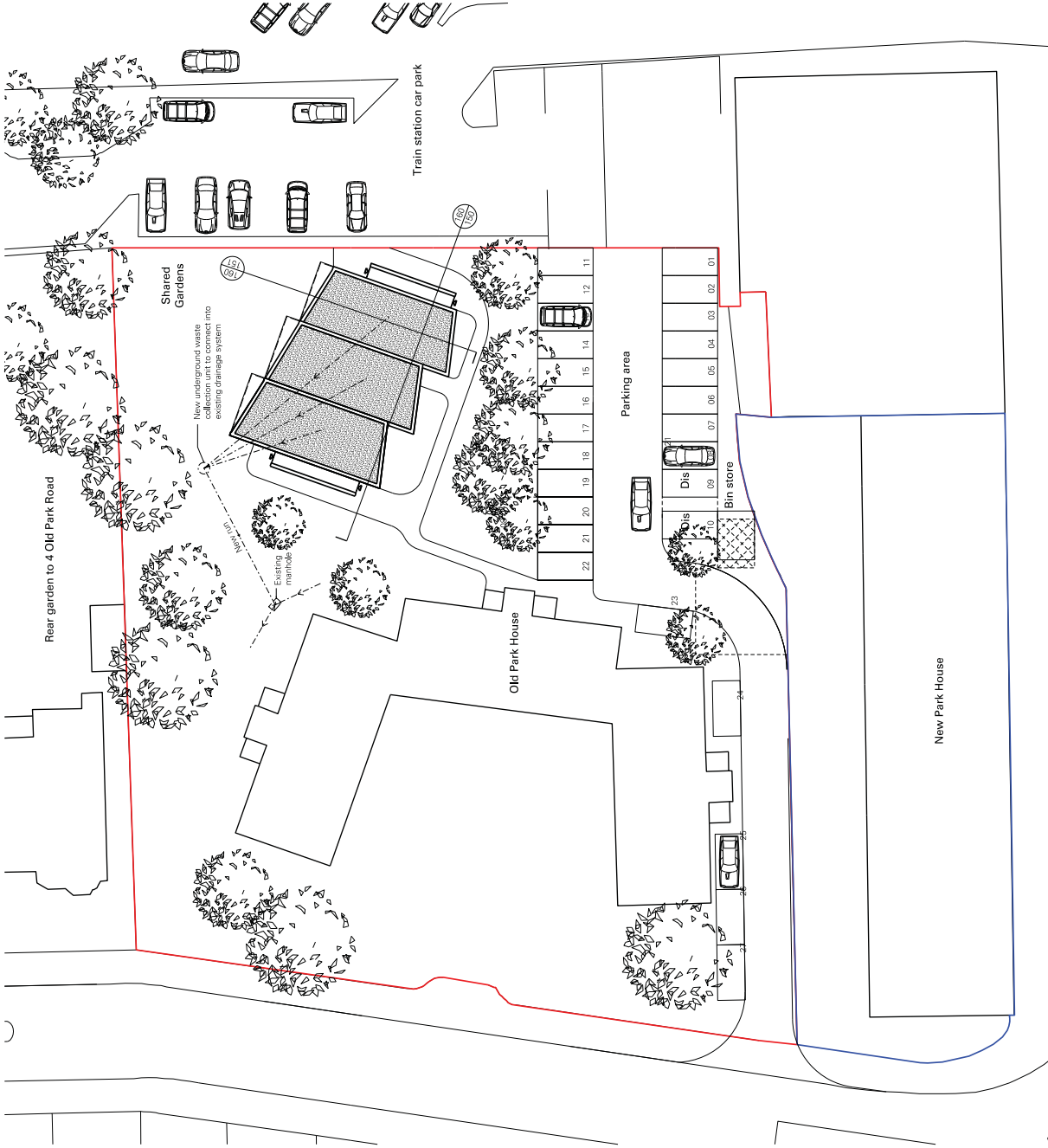
For proposed section A refer to 100 150
For proposed section B refer to 100 151

Planning

Drawing subtitle

Project	Old Park House, Old Park Road, London, N12 8BD
Drawing	Site plan
Scale	1:200 @ A1, 1:500 @ A3
Drawing number	020
Revision	C
Date	Apr 09
Revision	C
Author	W.G.
Client	121848888

Revision	Date	Drawn	Approved	Notes
A	27.04.2009	MJ	PKV	PRE-APP ADVICE
B	29.05.2009	MJ	PKV	GENERAL
C	01.07.2009	JA	PKV	PLANNING



General Notes

- 100.005 For proposed location plan refer to
- 100.006 For proposed site plan refer to
- 100.130 For proposed ground floor plan refer to
- 100.131 For proposed first floor plan refer to
- 100.132 For proposed 2nd floor plan refer to
- 100.133 For proposed car plan refer to
- 100.145 For proposed west elevation refer to
- 100.146 For proposed north elevation refer to
- 100.147 For proposed east elevation refer to
- 100.148 For proposed south elevation refer to

Parking and amenity

- 23 x 2.5m x 5.0m (standard) 785sqm
- Existing amenity 1200sqm
- Proposed new amenity 1000sqm
- Total amenity 1800sqm

Key

- Text to be Unvers Light



TP/09/1198



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Scale 1/1250 Date 1/12/2009

Centre = 536341 E 197026 N

Application Number: TP/09/1198 **Ward:** Enfield Highway

Date of Registration: 4th September 2009

Contact: David Snell 3838

Location: 1, JUTE LANE, ENFIELD, EN3 7PJ

Proposal: Change of use from snooker club (D2) to a night club / banqueting suite (Sui Generis).

Applicant Name & Address:

Mr Vedat Habiboglu
9, Hatton Road
Cheshunt
Waltham Cross
EN8 9QS

Agent Name & Address:

Mr Saddle Munthali, Kaseka Associates
74, MEDCALF ROAD
ENFIELD
EN3 6HL

Recommendation: That planning permission be **GRANTED**, subject to a legal agreement requiring that the use only operates with the provision of car parking proposed in a linked application, and subject to the following conditions:

1. The use hereby approved shall not be commence or operate until the car parking area proposed in a linked application is made available for visitor parking in accordance with Drawing No. KA/JL. The car park shall thereafter continue to be maintained for this purpose during operating hours of the premises unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the development complies with Unitary Development Plan Policies and does not prejudice parking conditions and the safety of traffic flow on adjoining highways.

2. Prior to the use hereby approved commencing a scheme detailing provisions to be made for the control of noise emanating from the building shall be submitted to and approved by the local planning authority. The scheme shall include a structural sound test to determine the likely breakout of noise and details of any structural measures to mitigate against noise emanating from the building. The approved details shall be implemented prior to the commencement of the use and shall thereafter be maintained.

Reason: To prevent noise pollution having regard to Unitary Development Plan Policy (II)EN30 and Planning Policy Statement 25.

3. Prior to installation or operation details of any air conditioning or mechanical air conditioning and ventilation plant shall be including a noise assessment report shall be submitted to and approved by the local planning authority. The plant shall thereafter be installed or maintained in accordance with the approved detail.

Reason: To prevent noise pollution having regard to Unitary Development Plan Policy (II)EN30 and Planning Policy Statement 25.

4. Prior to the use hereby approved commencing details of refuse storage to include the provision of secure lidded containers and a commercial contract for the collection of waste shall be submitted to and approved by the local planning authority. The approved detail shall be implemented prior to the use commences and shall thereafter maintained.

Reason: In the interests of safeguarding the satisfactory disposal of waste.

5. Prior to the use hereby approved commencing a scheme of lighting for the area of the site outside the public entrance shall be submitted to and approved by the local planning authority. The scheme shall be implemented before the use commences and shall thereafter be maintained.

Reason: In the interests of highway and pedestrian safety.

6. C51A Time Limited Permission

Site and surroundings

The site contains a former single storey industrial building that has been in use as a snooker club since February 1988. The immediate and surrounding area to the east comprises a primary industrial area. Brimsdown railway station is located to the immediate west with a predominantly residential area beyond. The nearest residential properties are located on Green Street and Brimsdown Avenue some 50 metres from the application site with the railway line in between.

Proposal

It is proposed to use the building as a night club/banqueting suite

Twenty five parking spaces to be proposed in a separate application.

Relevant planning history:

TP/87/1407 - change of use of premises from metal foundry to snooker club involving erection of a single storey extension at the front & provision of car parking spaces at side and rear – granted in February 1988.

TP/08/0836 – change of use to night club – refused in February 1988 or the following reasons:

1. The proposed use of the premises as a nightclub would result in the introduction of an inappropriate activity within a designated Primary Industrial Area, detrimental to the function, character and economic activity of the area. This would be contrary to Policies (I) GD1, (I)GD2 and (II) E2 of the Unitary Development Plan.
2. The proposal does not adequately address potential parking and vehicle trip generation and therefore could give rise to kerbside parking in the adjacent streets to the detriment of safety and the free flow of traffic including pedestrians and public transport traffic on the public highway. The proposal is therefore contrary to Policy (II)GD6 of the Unitary Development Plan.

TP/87/1407/VAR1 – Variation of condition 7 of TP/07/1407 to allow the use as a Banqueting Suite / Dance Hall was submitted on 22 July 2009 but was withdrawn.

Consultation

Public

Letters were sent to 377 neighbouring properties. 5 replies have been received from residents living in Osbourne Road, Goldsdown Close, Green Street and Enstone Road raising the following issues.

- Noise and disturbance
- Encourage illegal activity
- Encourage vandalism
- Encourage anti-social behaviour
- Traffic generation and on-street parking

Members are advised that one of the above letters refers to a petition that the writer signed opposing the application. A petition has not been received.

One letter of support has been received from a taxi driver who feels that the development will improve business opportunity in the area.

Internal

Environmental Health and Regulation raised no objection, subject to conditions.

External

None.

Relevant Policy

The London Plan

3C.23 Parking Strategy

Unitary Development Plan

- | | |
|----------|--|
| (I) GD1 | New development to have regard to its surroundings. |
| (I) GD2 | New development will improve the environment. |
| (II) GD6 | Traffic Generation |
| (II) E2 | Concentrate B1-B8 uses within Primary Industrial Area. |

Other

PPS6 – Planning for Town Centres
PPG 13 - Transport

Analysis

The main considerations associated with the proposed change of use are its potential impact on the surrounding environment, and its appropriateness within the Primary Industrial Area.

Appropriateness of use with the Primary Industrial Area

The site is located within a Primary Industrial Area. Policy (II)E2 seeks to concentrate activities falling within use classes B1-B8 in the Primary Industrial Area. The existing D2 use of the site falls outside of the B1-B8 use classes.

Reason for refusal 1 of TP/08/0836 was based on an assessment that the proposed use of the premises as a nightclub would result in the introduction of an inappropriate activity within a designated Primary Industrial Area, detrimental to the function, character and economic activity of the area. This would be contrary to Policy (II) E2 of the Unitary Development Plan.

However, in assessing this application it is considered that insufficient weight was previously given to the fact that the premises have not been used for industrial purposes since 1988 and that its use for recreational activity is longstanding. On that basis it is considered that there is a balance of acceptability having regard to Policy (II)E2. In addition the Design and Access Statement submitted with this application clarifies site ownership in that the building and car parking area are leased from the freehold interest of a larger site. Thus approval of this application should not prejudice the future redevelopment of this and the adjoining site for primary employment purposes.

Furthermore, in considering an application for change of use of the first floor of No.4, Princes Road, London, N18 (TP/07/1029) from warehouse to function hall at the meeting of Committee on 22nd September 2009 Members concluded that this similar use would not be compromised the industrial area and that there would be employment generated by the proposal. This application differs from the previous application in that it combines use as a night club with activities associated with a banqueting suite catering for functions such as weddings and similar meetings. In considering TP/07/1029 Members concluded that such activity supported the community.

Traffic generation and parking

A linked application proposes the provision of 25 parking spaces to be made available during opening hours for customers within a yard area adjoining the site currently used for storage.

The proposed car parking provision complies with the London Plan guidance of 1 space for 25 customers for this type of use. It is considered that the provision of the car parking space would provide an adequate number of bays for the club use and that this would overcome the previous reason for refusal.

The Design and Access statement states that the spaces will be available alongside the lease for the building during the opening hours of the club. However, the parking area is currently used for pallet storage in connection with an adjoining use and it is unclear how this arrangement will work in practice. Should planning permission be granted it should therefore be subject to a S.106 Agreement linking parking availability with the club use and a condition requiring that the car parking is made available during the opening hours of the club.

Impact on residential amenity

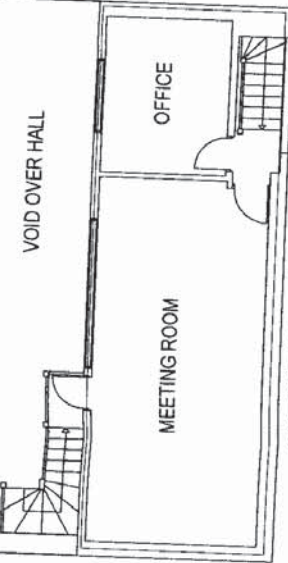
The nearest residential properties to the premises are located in Green Street and Brimsdown Avenue some 50/55 metres away on the west side of the railway line, Brimsdown station and the Isaac Walton public house. Given this distancing and intervening uses and buildings, subject to conditions in respect of noise generated from within the building the proposed use of the building as a nightclub would not have a direct adverse impact on residential amenity.

Conclusion

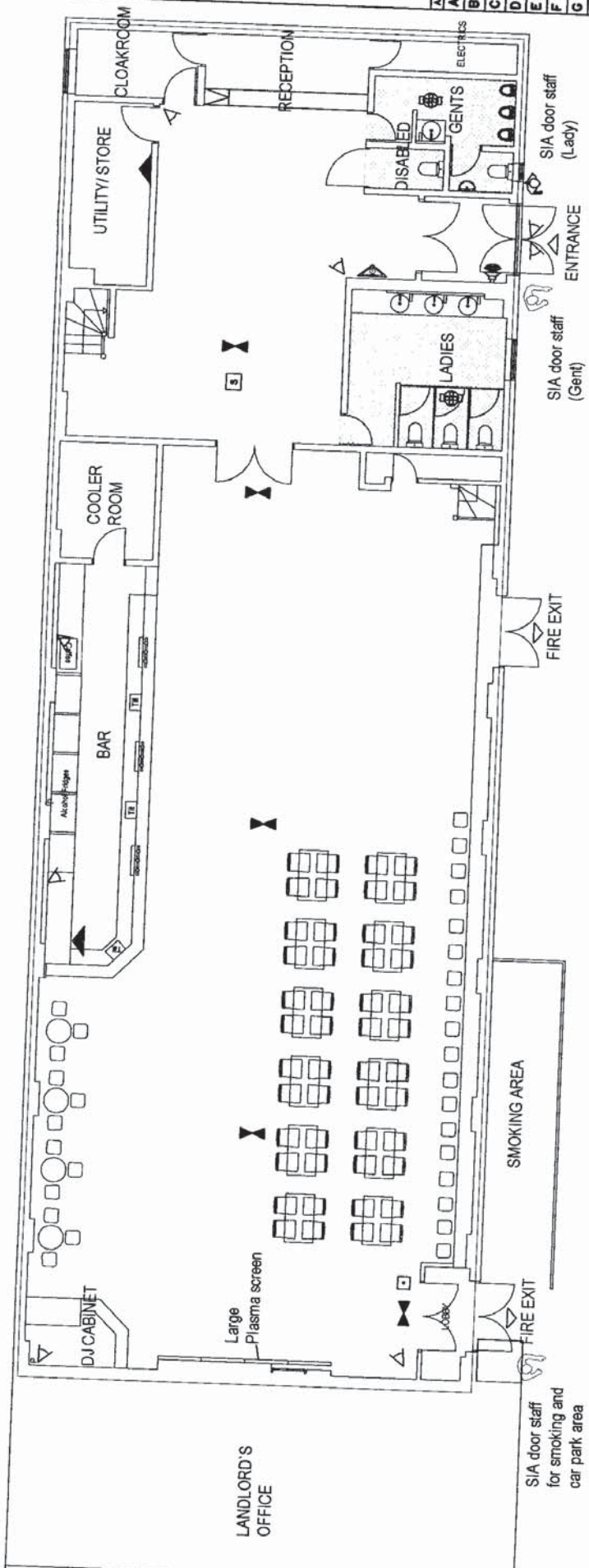
The application is recommended for approval for the following reasons:

1. Having regard to the existing longstanding use of the premises the proposed use of the premises as a nightclub would not result in the introduction of an activity into the Primary Industrial Area, that would be detrimental to the function, character and economic activity of the area. This would be contrary to Policies (I) GD1, (I)GD2 and (II) E2 of the Unitary Development Plan.
2. The proposed development due to the provision of parking within the adjacent site, the subject of a linked application, would not prejudice the provision of on street parking nor would it give rise to conditions prejudicial to the free flow and safety of traffic on the adjoining highways having regard to Policies (II)GD6 and (II)GD8 of the Unitary Development Plan, Policy 3C.23 of the London Plan and PPG 13 - Transport.

TR091198



FIRST FLOOR PLAN



LEGEND

- WC AREA
- FRIDGES
- AMBIT OF LICENSED PREMISES
- SAFETY LIGHTS
- SMOKE DETECTOR
- CCTV
- CARBON DIOXIDE FIRE EXTINGUISHER
- POWDER FIRE EXTINGUISHER
- FAN
- FIRE ALARM BELL
- FIRE ALARM CALL-POINT
- DISABLED EMERGENCY CALL-POINT & ALARM

LONDON BOROUGH OF ENFIELD
20 AUG 2009
DEVELOPER SERVICES

A	AMBIT OF LICENSED PREMISES
B	FRIDGES
C	SAFETY LIGHTS
D	SMOKE DETECTOR
E	CCTV
F	CARBON DIOXIDE FIRE EXTINGUISHER
G	POWDER FIRE EXTINGUISHER

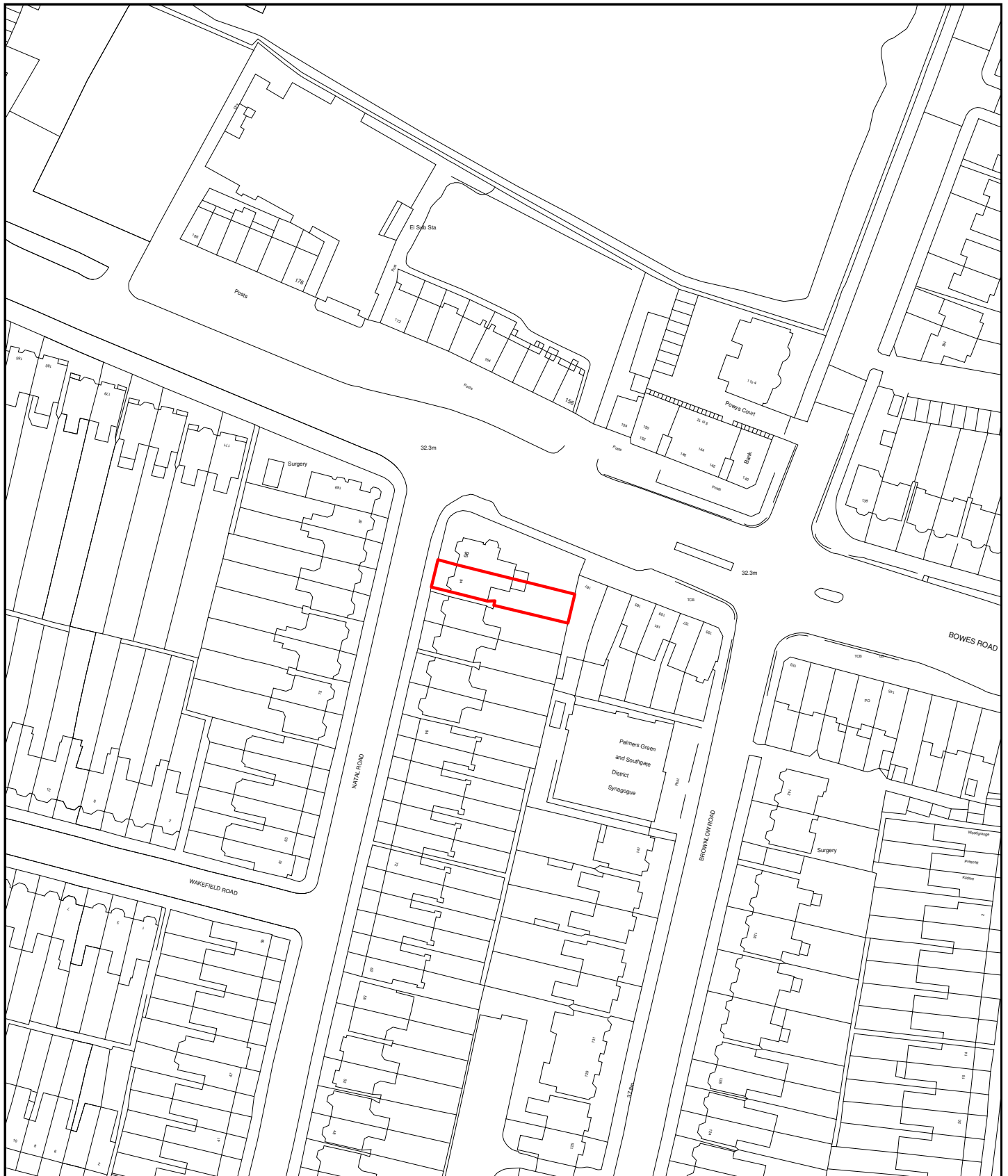
KASEKA ASSOCIATES
74 MEDFORD ROAD
ENFIELD, MIDDLESEX
EN3 8HL, Tel: 01892 716661
Mobile: 07636 580712
e-mail: kaseka@kaseka.co.uk

**PROPOSED NIGHT CLUB/
SINGLET SUITE**
1 LUFF LANE, BRIMSDOWN,
ENFIELD, EN3 7PJ

PROPOSED
GROUND AND FIRST FLOOR
PLANS
SCALE: 1:100
DATE: 12.08.09
DRAWN BY: [blank]
CHECKED BY: [blank]

KAJJL AB 1

PROPOSED GROUND FLOOR PLAN LAYOUT



Development Control



Scale - 1:1250
Time of plot: 16:24

Date of plot: 01/12/2009

Application Number: TP/09/1575 **Ward:** Bowes
Date of Registration: 16th October 2009

Contact: Robert Lancaster 4019

Location: 94, NATAL ROAD, LONDON, N11 2HY

Proposal: Conversion of single family dwelling house into 3 self contained units (comprising 2 x 2-bed and 1 x studio) involving a rear dormer window, alterations to rear side fenestration and new window to first floor rear (Renewal of approval granted under ref: TP/05/1603).

Applicant Name & Address:

Ms Aydin Turanli
94, NATAL ROAD
LONDON
N11 2HY

Agent Name & Address:

Mr Chris Gwilliam, Ideal Planning Permissions
1st Floor, Erico House
93-99, Upper Richmond Road
Putney
London
SW15 2TG

Note for Members

Although an application of this nature would normally be determined under delegated powers, due to concerns, amongst the community about further conversions, Councillor Rodin has requested that this be determined at Planning Committee.

Recommendation: That planning permission be **GRANTED** subject to the following Conditions

1. C08 Materials to Match
2. C25 No additional Fenestration
3. C59 Cycle parking spaces
4. C51A Time Limited Permission

Site and Surroundings

This is a 2-storey mid terrace property situated on Natal Road which is residential in character. The property has already been converted into 2 self-contained flats.

Proposal

Permission is sought for the conversion of the property into 3 self contained flats comprising two 2-bed and one studio units and involving a rear dormer window, alterations to rear side fenestration and new window to first floor rear.

Relevant History

TP/05/1603: Sub-division of single family dwelling house into 3 no. self contained units of accommodation (comprising 2 x 2-bed and 1 x studio) involving a rear dormer window, alterations to rear side fenestration and new window to first floor rear was approved in October 2005

TP/04/0670 - Conversion of existing property into 4 x 1-bed self contained flats incorporating a single storey rear extension and loft conversion with a rear dormer window; Refused subsequent appeal dismissed. The appeal was dismissed on the basis of the harm to residential amenity due to the proposed rear extension and the substandard size of the units proposed.

Consultations

Public

Consultation letters have been issued to 20 neighbouring properties. In addition, notice was displayed at the site. A letter of objection has been received signed by the occupiers of No.90 and 92 Natal Road, who raise the following concerns

sewage problems
overlooking from new window
noise and disturbance resulting from increased intensity of use
poor condition of property and front garden
security risk to neighbouring properties
poor sanitary conditions
increased pressure of parking in parked up area
loss of outlook, light and privacy

External: None

Internal: None

Relevant Policies

London Plan

3A.1 Increasing London's supply of housing
3A.2 Borough Housing Targets
4B.8 Respect Local Context and Communities

Unitary Development Plan

(I) GD1 Development to have appropriate regard to its surroundings.
(I) GD2 Development to improve the quality of the environment.
(II) GD3 Aesthetic and functional design
(II) GD6 Traffic generation.
(II) GD8 Access & servicing
(II) H1 To increase the housing stock in the Borough (see also Appendix A1.9)
(II) H8 Privacy

- (II) H9 Amenity space
- (II) H15 Roof Extensions
- (II) H16 Flat Conversions

Local Development Framework

The Planning and Compulsory Purchase Act 2004 requires the Council to replace the UDP with a Local Development Framework (LDF). The LDF Core Strategy will set out the spatial vision and strategic objectives for the Borough. The Core Strategy is now at an advanced stage in its adoption process and as this process continues, the weight given to it will grow and the relevant objectives are reported to demonstrate the degree to which the proposals are consistent with the emerging policy direction.

- SO3 Protect and enhance Enfield's environmental quality;
- SO5 Housing and People
- SO7 Distinctive, balanced, and healthier communities

Other Material Considerations

- PPS1 Delivering Sustainable Communities
- PPS3 Housing

Supplementary planning guidance on flat conversions

Analysis

Principle

The proposal would be compatible with Policies 3A.1, 3A.2 and 4B.3 of the London Plan insofar as it provides an addition to the Borough's housing stock which actively contributes towards both Borough specific and London-wide strategic housing targets. The residential use would also be compatible with the prevailing composition of the area

Impact on Residential Character

Policy (II)H16 considers the conversion of single dwellings into flats, with Appendix A1.9 outlining standards relating to these conversions stating'

"Where the number of residential units has reached or exceeded 20% of the total residential buildings in the road, then there will be a presumption against further conversions being permitted."

However the property's conversion into flats has already been approved under ref TP/04/0670 and the property has been occupied as two flats for a number of years. The proposal does not therefore involve any loss of single family dwelling house or further erosion of the existing family housing stock.

Furthermore, in assessing this proposal, weight must be given to the appeal decision made in respect of TP/04/0670 which although supported the Council's refusal, only upheld the decision on grounds relating to the impact of the rear extension and the inadequacy of the internal floor areas of the 4 units proposed. Mindful of this, planning permission was granted for three flats (ref: TP/05/1603). Consequently, subject to detailed matters set out in Policy (II)H16 being acceptably addressed, it is considered the use of the property as three flats is acceptable.

Accommodation Size & Internal Stacking

Ground floor flat consists of the following	First floor flat consists of the following -	Second floor flat consists of the following -
Kitchen/Living Room (open plan) - 25 sq.m	Kitchen /living Room (open plan) - 27 sq.m	Studio Room - 35 sq.m
Bedroom 1 - 15 sq.m	Bedroom - 15 sq.m	
Bedroom 2 - 12 sq.m	Bedroom - 15 sq.m	
Bathroom - 6.5 sq.m	Bathroom - 6.5 sq.m	Bathroom - 6 sq.m
Hallway - 8 sq.m	Hallway - 5 sq.m	
Total - 65.5 sq.m	Total - 67.5 sq.m	Total - 41 sq.m

Supplementary planning guidance requires that in the case of studio flats that the minimum net internal floor space of the converted accommodation should be 30 sq m in the case of studio and 57 sqm in the case of two bedroom flats. All 3 flats meet these requirements. It is also noted that approx 10 sq m of the floor space within the studio flat will be lost due to insufficient head height, however, this unit will still be of a sufficient size. Furthermore the internal stacking of rooms is acceptable.

Rear Dormer Window

The proposed rear dormer would be 2.7m wide, 2.8m deep and 1.8m high. In addition, it would be positioned 0.6m above the eaves, 0.4m below the ridge and set in a minimum of 1m from both sides.

Policy (II) H15 of the Unitary Development plan specifies that rear dormers should only be accepted if they are:

- of an appropriate size and location within the roof plane;
- in keeping with the character of the property; and
- not dominant when viewed from the surrounding area.

Taking into account its size and position within the roof, it is considered that the rear dormer is acceptable and would not appear incongruous or out of character with the appearance of the property or the surrounding area

In terms of residential amenity, the proposed dormer will face the flank wall of 167 Bowes Road, which is approx 19m away. It is considered that the proposed dormer window would not give rise to overlooking or an unacceptable loss of privacy.

First Floor Window

A new window is proposed in the first floor rear elevation of the projecting two storey rear addition. Measuring 0.8m wide x 1.2m high, this window will serve a first floor bedroom but due to the depth of the rear garden, will not lead to any loss of privacy to neighbouring properties.

Amenity Space

With regards to amenity space, Policy (II) H9 and Appendix A1.7 are applicable. The amenity space is provided for the use of the ground floor flat, which has direct access and is approx 90 sq.m. This provision is considered acceptable.

Parking

No off street parking is proposed or exists for the current occupation of the property as two flats.

The previous application which proposed the conversion to 4 no. flats (ref TP/04/0670) was refused on the basis of not providing any off street car parking. However at appeal, whilst the Inspector noted that on-street parking levels high he commented

"but PPGs 3 and 13 are clear, and the appellant's reference to easy access here to comprehensive public transport facilities is fair. There is no specific indication in the cases against the appeal project of what particular elements of danger or impedance to free traffic flow would arise if the appeal project were to proceed. That applies both to the very busy Bowes Road (part of A406) and the residential Natal Road."

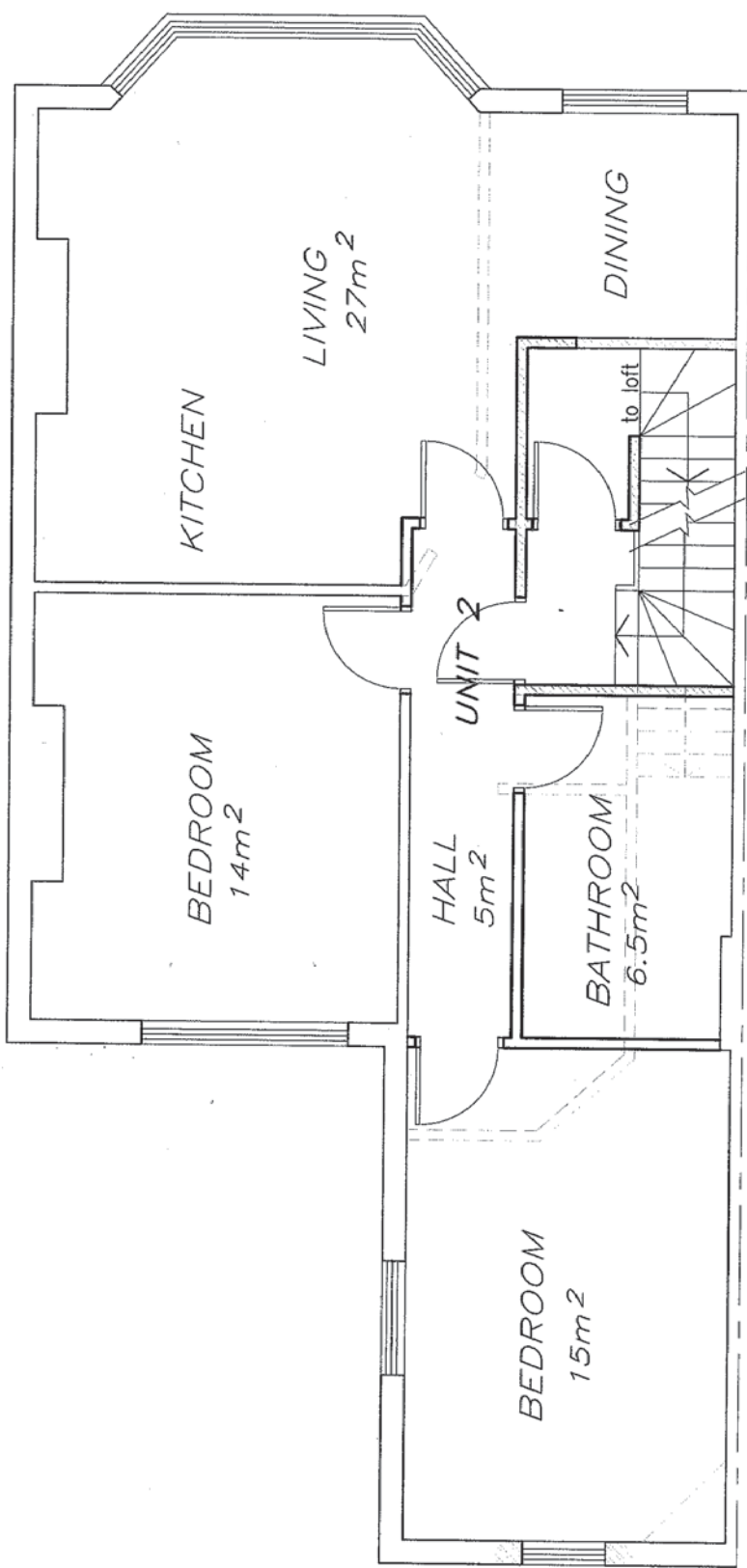
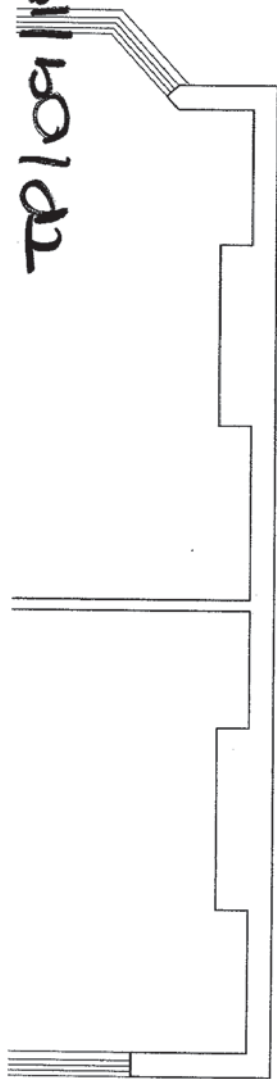
Subsequently the appeal was not dismissed on this basis and therefore it is considered that it would be difficult to sustain an objection on this ground, particularly as the number of units has been reduced.

Conclusion

Having regard to the previous decisions, and noting the floor areas of the individual flats units and the removal of the rear extension from this proposal, it is recommended that planning permission is approved for the following reasons:

1. The proposed conversion of the single family dwelling into 3 no. self contained units, having regard to the floor space, internal stacking and amenity space, does not detract from the residential character and amenities of the surrounding area and in particular, the amenities of neighbouring residential properties, in accordance with Policies (I)GD1, (II)GD3, (II)H9 and (II)H16 of the Unitary Development Plan and Supplementary Planning Guidance: Flat Conversions.
2. The proposed conversion of the single family dwelling into 3 no. self contained units in light of the close proximity to public transport and taking into account the Inspectors decision in the relevant appeal reference APP/Q5300/A/04/1159672 would not give rise to conditions prejudicial to the free flow and safety of traffic on the adjoining highways having regard to Policy (II)GD6 of the Unitary Development Plan.
3. The conversion into three flats, would contribute to increasing the housing stock, as well as providing units of an acceptable internal size and layout having regard to policies (I) GD1, (I) GD2 and (II) H16 of the Enfield Unitary Development Plan and Supplementary Planning Guidance on Flat Conversions.

TP10911575



PROPOSED FIRST FLOOR PLAN
UNIT 2 - 2 BED FLAT

TOTAL AREA = 67.5m²

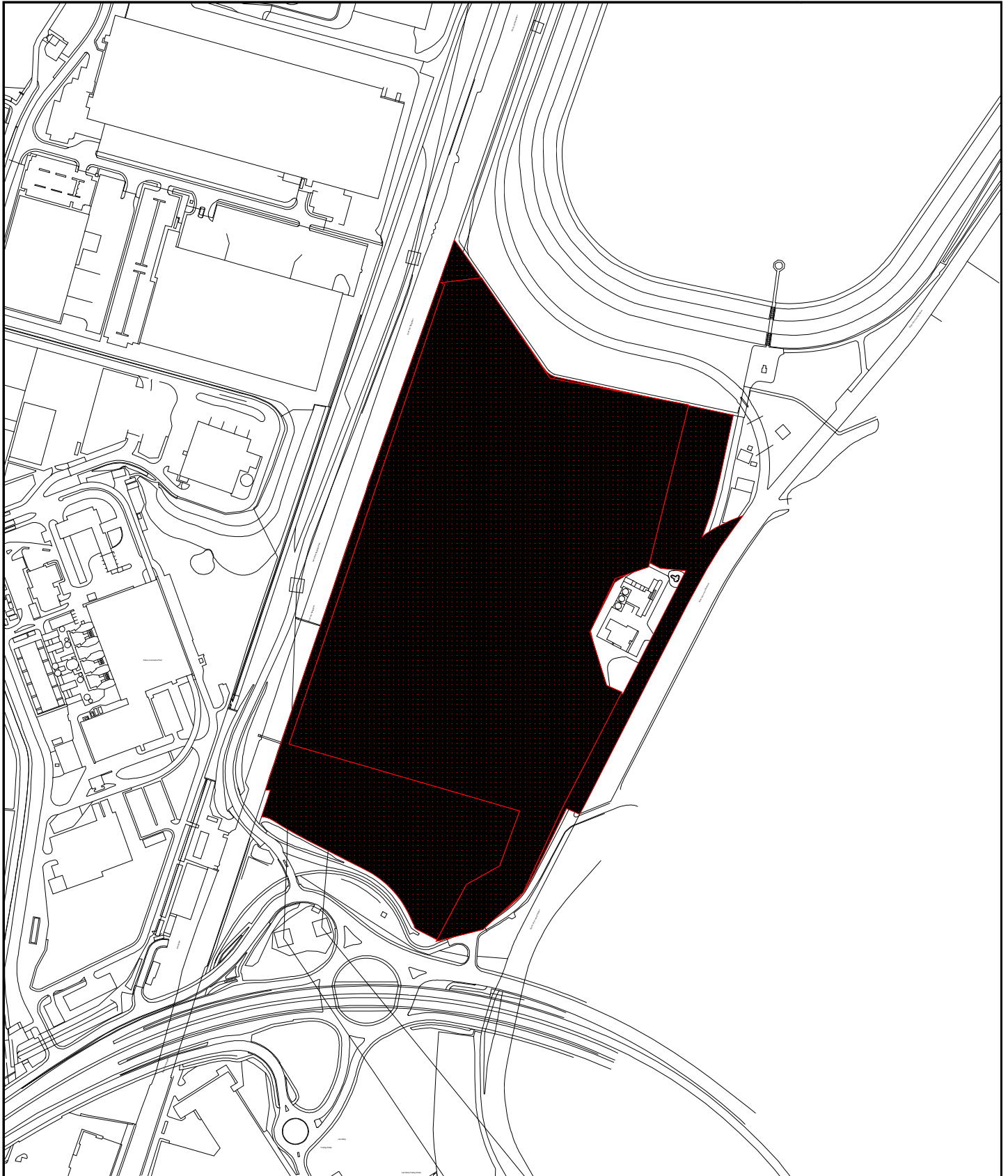
LONDON BOROUGH OF ENFIELD
16 OCT 2009
DEVELOPER SERVICES

Job Title
94 Natal Road
London N11 2HY
Proposed Conversion of
Property to 2No. 2 Bed Flat
& 1No. Studio (within Loft)

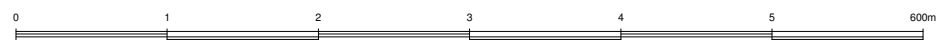
Dwg. Title
PROPOSED
FIRST FLOOR PLAN
UNIT 2 (2 Bed Flat)

Rev.	Date	Revision

Scale	1:50
Date	Sept 2005
Drawn by	EES
Dwg. No.	94NR/05/04
Rev.	



Development Control



Scale - 1:5000
Time of plot: 15:48

Date of plot: 01/12/2009

Application Number: TP/96/0150/3
Date of Registration: 10th May 2000

Ward: Edmonton Green

Contact: Andy Higham 3848

Location: Land to the south of William Girling Reservoir, Lower Hall Lane, Chingford, London, E4

Proposal: Continued use of land for recycling purposes to include crushing, screening and stockpiling of concrete and other recyclable materials, erection of pole mounted CCTV and provision of temporary buildings for site management. (Approved under Ref:TP/96/0150 - Renewal of consent)

Applicant Name & Address:

Thames Water Property
Reading Bridle House
Reading
Berkshire
RG1 8PR

Note for Members

This is an application submitted by Thames Water to permit the continued use of the land for recycling purposes to include crushing, screening and stockpiling of concrete and other recyclable materials together with the erection of a pole mounted CCTV and provision of temporary site buildings.

Planning permission was initially granted in 1997 (ref: TP/96/0150) for a three year period which expired in June 2000. This application was submitted to extend this period and at a meeting of Planning Committee in January 2003, it was resolved to grant conditional planning permission subject to the completion of a S106 legal agreement. The permission would have permitted the continued use of the site for a further 6 year period with the legal agreement controlling the phased cessation of the use and the restoration of the site to an appearance compatible with the sites designation as Green Belt within the Lee Valley Regional Park.

In particular, the legal agreement was to specify requirement for :

- a) a detailed ground investigation
- b) a phasing programme for the proposed restoration works;
- c) a methodology for the clearance of site operations;
- d) a commitment to the removal of building
- e) a commitment to reinstatement of sub soil and top soil layers
- f) a commitment to a scheme of landscaping with details of grass seeding.

Although it was acknowledged at the time of this application that the proposed continuation of the recycling uses on a permanent or long term basis would represent an inappropriate use contrary to policies relating to the Green Belt, it was considered that on balance, given that the operation supported recycling objectives, the continued use for a further limited period of 6 years was acceptable as part of a structured approach to the cessation of the use and restoration of the sites appearance.

Unfortunately, despite extensive negotiations on the legal agreement regarding the phased withdrawal of operations from the site, agreement on the specification for the restoration works

was never reached and the planning permission never issued. In the interim, the recycling operation has continued.

However, it remains a long term objective to seek the restoration of this site to a use and condition more appropriate to its location within Green Belt. This view is supported by the LVRP. Furthermore, the appearance of the site is unattractive and detrimental to the appearance of the surrounding area which includes the William Girling Reservoir, parts of the Lee Navigation and the Lee Valley Regional Park.

In the intervening period, the local policy context has materially altered with the site additionally now falling within the Central Leaside Area Action Plan. This emerging LDF document would foster the comprehensive regeneration of the Central Leaside Area which includes the application site. Within this AAP, it is proposed that the site contributes to the improvement in the local environment through maximising the value derived from its Green Belt designation and location within the Lee Valley Regional Park. In addition, its undeveloped state will serve as an area for possible flood compensation to support other development essential to the regeneration of the Central Leaside area.

In the light of this emerging policy and responding to the fact that the Council needs to retain control over the future of the site to ensure the present use does not prejudice delivery of these emerging policy and place shaping objectives, it is proposed to refuse planning permission. This will enable the Council to serve an enforcement notice to stop the unauthorised use of the site. Failure to take positive action by June 2010 would enable the applicant to claim the use is lawful after a 10 year period where it has operated without consent which would result in the Council losing control over the future of the site.

Notwithstanding the above, it is possible a negotiated solution with Thames Water and the operator can be reached to secure a phased withdrawal from the site but this would need to be separate application supported by an agreed legal agreement. To assist, a copy of the original report considered by Members is attached.

Recommendation: It is recommended that planning permission be **REFUSED** for the following reason:

The continued use of the site as a waste transfer facility represents an inappropriate form of development and together with its appearance, detracts from the open character and visual amenities of the Green Belt and the Lee valley Regional Park (Area of Special Character). This is contrary to Policy (II)G1, (II)G11, (II)G20, (II)G30 of the Unitary Development Plan, Policy 3D.9 of the London Plan as well as the objectives of PPG2 "Green Belt"

Previous Report

Site and Surroundings

The application site comprises 10.82 ha. of open land to the south of William Girling Reservoir owned by Thames Water Authority. Access is gained from Lower Hall Lane off the North Circular Road (A406). The site lies within the Green Belt and partly within the Lee Valley Regional Park and adjacent to part of the Green Belt located within the LB Waltham Forest. The River Lee Navigation runs along the western boundary whilst part of the River Lee (Diversion) runs adjacent to the eastern boundary.

Amplification of Proposal

The application seeks renewal of a temporary consent for a period of 6 more years;-

- a) the importation of waste soil and demolition materials which are screened according to particle size to produce a range of re-useable aggregate materials for sale suitable for construction and road building purposes,
- b) crushing and re-cycling of concrete,
- c) provision of 3 single storey temporary buildings for management purposes,
- d) provision of 9 CCTV cameras mounted on a 5.3 m. high pole operated by infrared sensors

Relevant Planning Decisions

There have been several decisions relating to part of the application site which partly fell within the L.B. of Waltham Forest and partly within the L.B. Enfield. The whole site now lies entirely within Enfield following re-organisation of Borough boundaries.

(1) WF/88/1383 - mixing and blending of materials to produce topsoil - granted consent on 08.02.89.

(2) TP/88/1783 - the importation of dried sludge from Thames Water, blending with soils and sub soils for the purpose of producing quality assured topsoil and the erection of temporary buildings for site management purposes, granted temporary consent on 07.02.89.

(3) WF/92/0102 - continued use for the mixing and blending of materials to produce topsoil granted consent on 14.07.92.

(4) TP/92/0152 - renewal of temporary permission (Ref. TP/88/1783), granted consent on 25.06.92.

(5) TP/92/0152/1 - renewal - production of topsoils, granted consent on 19.10.95.

(6) TP/96/0150 - Use of land for recycling purposes to include crushing, screening and stockpiling of concrete and other recyclable materials, erection of pole mounted CCTV and provision of temporary buildings for site management for a period of three years, granted consent on 30.07.1997

Consultations

Public

None were required to be consulted having regard to the remote location of the site. However, the application was advertised a Departure on site and in the local Press. One letter received from the Enfield Preservation Society who raise no objection to renewal of consent on a temporary basis. However, they consider consent should not become permanent to allow the Local Authority to retain control over the disruptive use.

Internal

The Assistant Director (Community Protection) raises no objection subject to re-imposition of conditions relating to hours of operation and provision of wheel-washing facilities. In addition, it is confirmed that the concrete crusher has authorisation under the provisions of the Environmental Protection Act 1990.

Cleansing advises that the site is of **strategic** importance for recycling for several London Boroughs including Enfield and Waltham Forest. It is shortly to be used by the London Boroughs of Islington and Haringey. The estimated tonnage deposited by the borough for Year 2002/3 is

over 9,000 tonnes and will increase to over 10,000 tonnes in Year 2003/4. There is no alternative recycling site in this part of London and the nearest landfill site is in Cambridgeshire. Thus the closure of the site would necessitate removal of large quantities of material to landfill at more distant locations at considerable cost and by unsustainable means. Additionally, this would greatly increase traffic movements on roads inside and external to the Borough with detrimental environmental effects.

External

British Waterways have no objection in principle subject to further planting on the bund to soften its appearance and provision of further screening against noise and dust. They also request imposition of a condition limiting the height of the operation.

English Heritage has advised that any archaeological requirements can be waived.

The Environment Agency has advised that they have no objection to the renewal of planning permission for a temporary period. They advise that Camden Plant Limited currently hold a Waste Management Licence for screening waste soil and demolition materials, whilst the concrete crushing aspect of the operation is exempt from the Waste Management Licensing Regulations 1994. The operator has a good record of compliance with the conditions of the Licence. The imposition of three conditions relating to the requirement of an annual topographical survey of the area being restored to agree floodplain levels, the construction of storage facilities for oils, fuels or chemicals and an assessment of landfill gas, is requested.

No observations have been received from the London Canals Committee.

The London Borough of Waltham Forest has objected on the grounds that they consider the use inappropriate in the Green Belt. Additionally, that the proposal represents a significant intensification of existing activities, detrimental to the amenities of neighbouring areas of Waltham Forest. Furthermore, they consider that granting a 10 year temporary consent will make it extremely difficult to refuse any further application and again raise objection to continued long term use of the site for waste re-cycling. If however, the Council is minded to grant permission, restoration of the flood plain and enhancement of the nearby nature conservation sites must be ensured.

The Lea Valley Regional Park Authority (LVRP) has advised that they would object to permanent use of the site for re-cycling purposes. They consider this is an important site in the Regional Park in landscape and recreational terms. Despite the continued detrimental effect on the appearance and amenity of the area, the LVRP Authority would raise no objection to the continued use for a three-year temporary period. However, in the event of the Council being minded to grant permanent permission, the LVRP would request the Secretary of State to call-in the application for determination. A further letter has also been received objecting to the restoration and enhancement of the area for a longer temporary period of 10 years. Furthermore, they would wish to see that the entire site is restored to an attractive condition and to levels which enhance the appearance of the area and its value for birds and wildlife.

English Nature have advised that they have no objection to the renewal of the application provided that the condition relating to lighting is re-imposed. They would also wish to be consulted regarding any future proposals for the site to safeguard the populations of migratory birds that visit the site.

Transport for London raise no objection subject to a condition requiring wheel-washing facilities as mud has been carried onto the A406 trunk Road.

The North London Waste Authority (NLWA) support the application on the grounds that the site is the only construction and demolition waste recycling plant in the North London Waste Authority area. If the site were closed construction waste from Civic Amenity sites would be transported greater distances including to landfill sites resulting in considerable environmental impact resulting from increased lorry movement and additional costs to North London residents. NLWA advise that they are developing a 15 year strategy including a "Waste Plan" to assess future waste management and land planning requirements. The availability of the site would contribute to ensuring that the North London Partners are self sufficient in facilities for recycling construction wastes.

Appropriate UDP Policies

- (I) G1 - To support strongly the principle of the Green Belt by maintaining the Green Belt boundaries and resisting inappropriate developments
- (I) G2 - To seek the improvement and enhancement of the environment within the Green Belt
- (II) G1 - To resist inappropriate uses within the Green Belt except in very special circumstances
- (II) G5 - To seek to restore, manage and enhance the landscape of the Green Belt
- (I) GD1 - Surroundings and integration of development.
- (I) GD2 - Quality of life and visual amenity.
- (II) G30 - Development adjacent to the Lea Valley Regional Park
- (II) E2 - Industrial uses located in appropriate areas.
- (II) EN27 - Maintain an effective and environmentally beneficial system of waste collection, treatment, and waste disposal, in co-operation with the waste disposal authorities, having regard to the guidelines for waste disposal planning drawn up by Serplan
- (II) EN28 - To make provision for sites catering for the transfer or disposal of waste materials.
- (II) EN29 - To ensure the recycling of as much waste material as possible.

Interim UDP Policy

None

National Policy

PPG2 : Government's Waste Strategy 2000 for England and Wales, May 2000

MPG6: Guidelines for Aggregates Provision in England, 1994.

RPG9, March 2001, England,

PPG10 Planning and Waste Management, September 1999

Municipal Waste Management Strategy, Consultation Draft - GLA, Mayor for London - July 2001

Economic Development Implications

Any comments will be reported at Committee.

Analysis

Appropriateness of the use within the Green Belt

The site is located within the Green Belt and the Lee Valley Regional Park. The appearance of the site from areas outside it and more distant locations is unattractive and detrimental to the appearance of the surrounding area which includes the William Girling Reservoir, parts of the Lee Navigation and the Lee Valley Regional Park. It is a long term objective to seek the restoration of this site to a use and condition more appropriate to its location within Green Belt. This view is supported by the LVRP who are interested in using the site for recreational purposes but have not obtained the necessary funds so far to either purchase or lease the site although these intentions have been outlined in the LVRP Plan.

The original consents were for an ancillary use to the Sewage works and appropriate to a public utility. The new recycling use approved for the 3 year temporary period under ref. TP/96/0150 is rather similar since it involves the importation of sub-soil but differs in that it involves a range of demolition materials including crushing and recycling of concrete, screening and storage and is a more commercially based recycling facility which appears to have intensified over the years.

The proposed continuation of the recycling uses on a permanent or long term basis would represent an inappropriate use and be contrary to policies relating to the Green Belt. The adjoining London Borough of Waltham Forest has raised objections to both this and the previous application on these grounds.

In support of the use there are numerous national and regional planning advice as well as emerging policy in the form of the Draft London Plan regarding recycling and re-use of materials. Comments have also been received from Cleansing and Waltham Forest who highlight their use of the site for treatment of their own waste materials as do a number of other London Boroughs. In particular, Enfield sends 10-15,000 tons per year of construction waste and waste from works on the public highways to this site alone. Where this site to cease operating, these users would be obliged to send their waste to more distantly located landfill sites incurring greater costs which would not be a sustainable method of treating such materials and which are currently being recycled on the application site.

The need for appropriately located recycling facilities within urban areas is not disputed but the continued use of the site in the Green Belt is also contrary to local, regional and national policy. On balance, taking into account the observations of existing users, the continued use is considered to be unacceptable and therefore any assessment of this proposal should include the means by which the use of the site can be stopped and it returned to a condition more appropriate to its location.

The Applicant initially sought a period of 10 years. However, through negotiation, this period has been reduced to 6 years which is considered the minimum necessary to effectively manage the closure of the site and its reinstatement involving the removal of material to agreed ground levels. Further temporary bunds to screen the site however, are considered appropriate in the short term to screen the site. Against this are those who feel any long-term permission would prejudice our ability to control the use of the site. However, in the circumstances, it is considered that this is a reasonable approach to take.

Means of Control

The operator holds a Waste Management Licence for screening waste soil and demolition materials and has a good record of compliance with the conditions of the Licence which is monitored by the Environment Agency (EA). The EA are anxious to see that ultimately the site is returned to original ground levels for environmental reasons. The Council under the requirements of the Environmental Protection Act 1990 monitors the concrete crushing aspect of the operation.

In addition, the use of the site is controlled by a number of conditions including a requirement for a temporary screening bund around part of the site to a height of 5m. This has been substantially planted and shows signs of mature growth in part. A condition was also imposed restricting the height of the stockpiles to 4m, i.e. 1m below the height of the bund so that any machinery working on top of them would not be seen from more distant areas from the site. Additionally, a wheel wash machine was installed following imposition of a condition.

It should be noted that there have been several complaints regarding the operation of the site which have usually been rectified immediately. The complaints relate to lack of dust control during limited periods of dry weather and the height of the stockpiles. Additionally, there have been occasions when mud has been transferred onto the trunk road. To address this, it is proposed that a condition be imposed requiring hard surfacing of part of the access road close to the existing wheel wash machinery to improve this situation and to which the applicant has agreed.

Notwithstanding the co-operation of the operators in addressing most of the previous identified problems, the use still generates considerable concern in terms of its visual impact on the Green Belt and the Lee Valley Regional Park, and the hindrance to the longer term development of the area for more appropriate uses.

Conclusion

The long-term objective has been to return the area to an appearance in keeping with the appearance and character of the surrounding area thus reflecting the UDP designations of the site.. The use was only envisaged as being as acceptable in the short-term due to the sensitive nature of the location within the Green Belt and the Lee Valley Regional Park. It is noted though that prior to the temporary use of this site being approved in 1997, there had been several temporary consents for mixing and blending of materials to produce topsoil since 1989. Furthermore, it is understood that the site was used for storage of spoil from the excavation of the William Girling Reservoir prior to this date and may have been used for landfill even earlier.

Notwithstanding the above, the time period for this application has been reduced including time to restore the site. In accepting this time frame it is suggested that the application be accompanied by a S106 agreement to guarantee the delivery of the stated objectives for this site. To this end, the following Heads of Terms are proposed at this stage, which have been discussed in conjunction with the Lee Valley Regional Park and the Environment Agency.

The Heads of Terms include the following matters: -

Cessation of current use

Site investigation

Phasing, timetable and method of clearance of site operations including removal of spoil heaps

Application of subsoil and topsoil layers

Removal of bunding, car park and access road

Landscaping including grass seeding

Any further details will be reported verbally to your Committee

Taking into account the above, the application is recommended for a further temporary consent subject to conditions.